

WATERTOWN DESIGN GUIDELINES + STANDARDS



PUBLIC MEETING 1

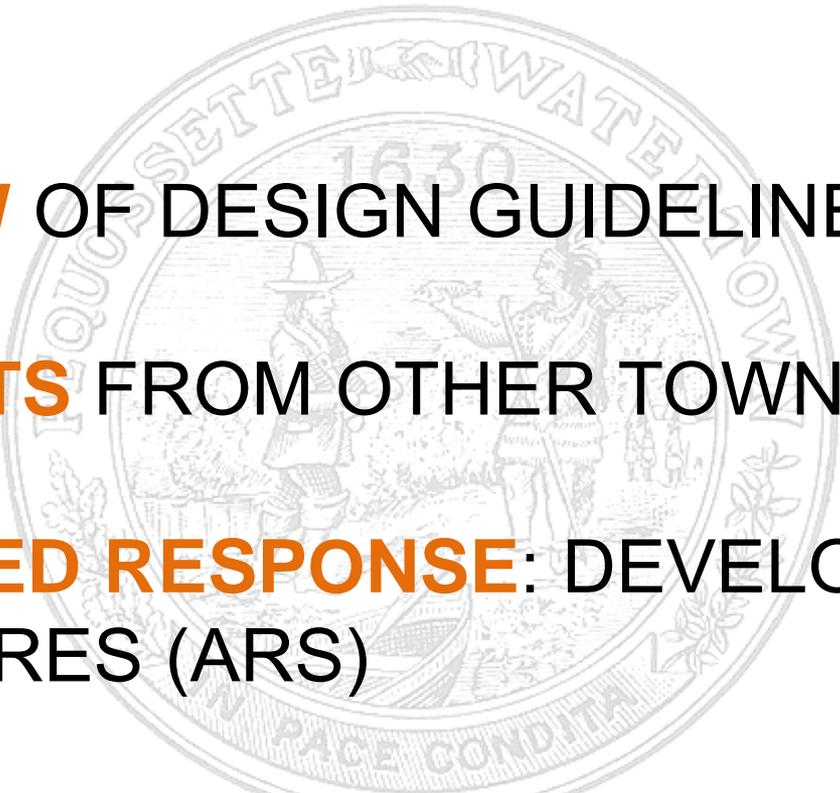
Watertown Middle School

October 15, 2014

architecture
urban design

GAMBLE
ASSOCIATES

WATERTOWN DESIGN GUIDELINES + STANDARDS

- 
- The seal of the Town of Waterbury, Vermont, is faintly visible in the background. It is circular and features a central figure holding a staff, surrounded by the text "WATERBURY VERMONT 1630" and "PACIS CONDITA".
- I. OVERVIEW** OF DESIGN GUIDELINES
 - II. HIGHLIGHTS** FROM OTHER TOWNS + CITIES
 - III. AUTOMATED RESPONSE:** DEVELOPMENT PRESSURES (ARS)
 - IV. FRAMEWORK** FOR MOVING FORWARD

WATERTOWN DESIGN GUIDELINES + STANDARDS



Development is often seen as a **negotiated tradeoff** between private development interests and amenities and benefits that are in the public good.

DESIGN GUIDELINES

Balancing act

Public Good:

Development impacts everyone... traffic, aesthetics, public space, shadows, view corridors



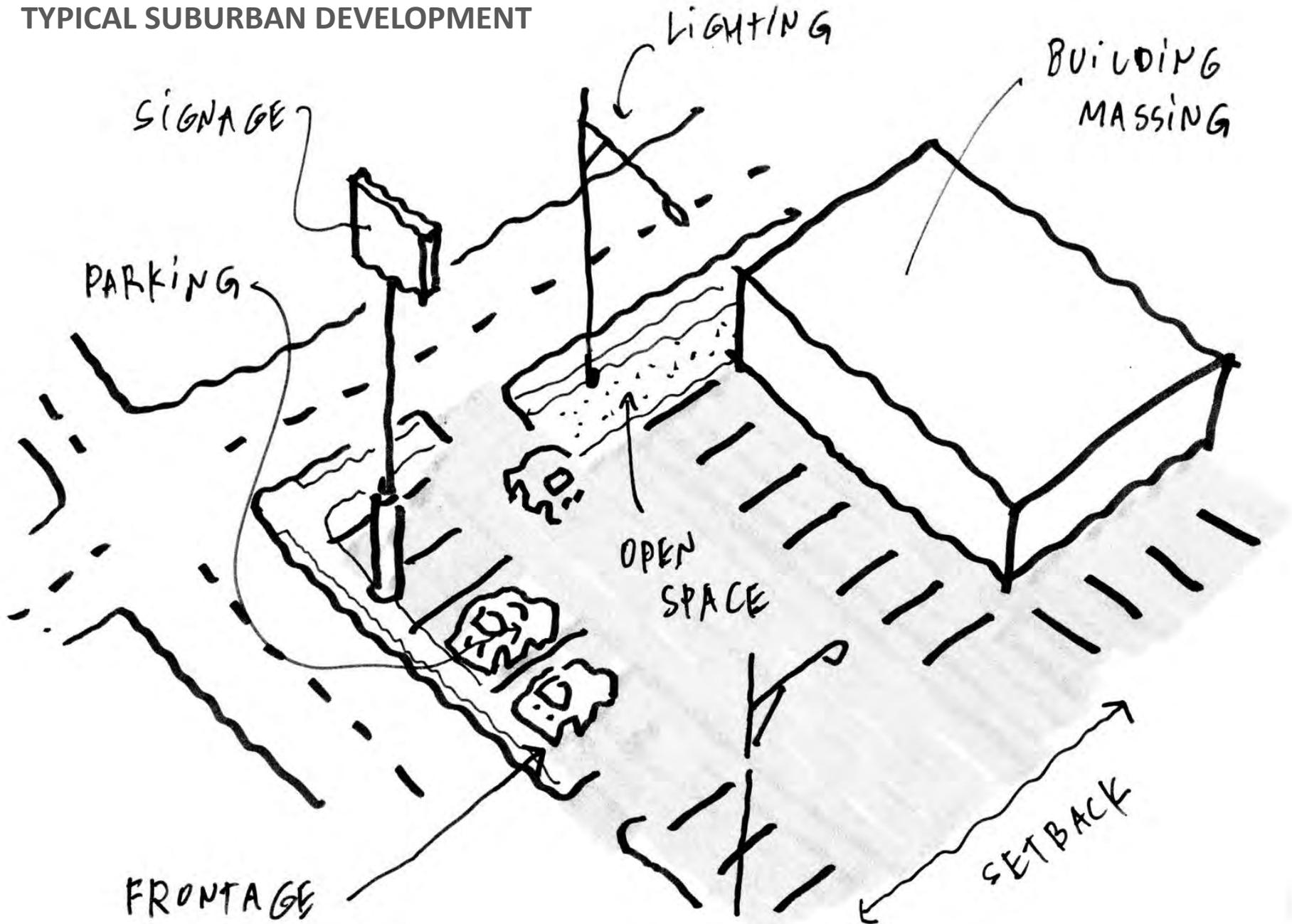
Private Rights:

One should have maximum development potential and flexibility on one's own property

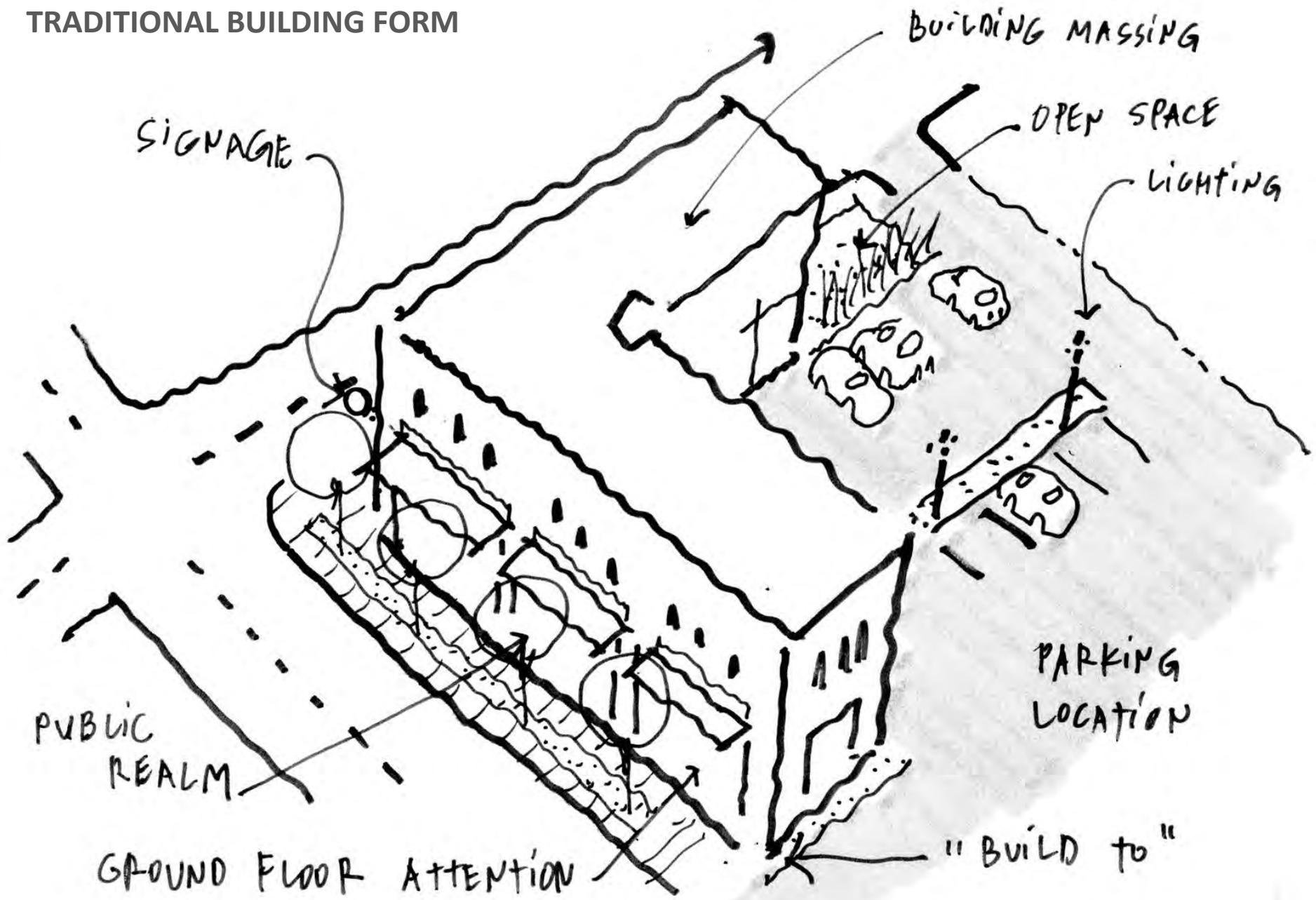
Walgreens in Arlington along Mass Ave.



TYPICAL SUBURBAN DEVELOPMENT



TRADITIONAL BUILDING FORM





Mt. Auburn Street

FUTURE REDEVELOPMENT SITE



Mt. Auburn Street and Arlington

permitted in the Central Business (CB) district.

- (d) **Dimensional Criteria:** The Uses above shall be subject to the following criteria: All dimensional criteria applying to the Central Business District.
- (e) **Minimum Site Plan Requirements:** Site Plan Requirements shall be the same as those required in the Central Business district.
- (f) **Parking Requirements:** Parking requirements for the Design Overlay District shall be the same as those required in the Central Business District.
- (g) **Regulated Building Facade Alterations**
 - (1) Regulated facades shall include all buildings within the Watertown Square Design District (see map of area).
 - (2) A regulated alteration or facade improvement shall be defined as any change in the visual appearance of the facade including any change in signage, lighting, door or window style, unless the change consists of an exact replication in terms of size, color, location, and detail of the replaced element.
 - (3) All regulated facades shall be subject to the design review process of Section 7.02 of the Zoning Ordinance.
 - (4) In reviewing all proposed facade alterations within the Watertown Square Design Overlay District, the Planning Board Staff's recommendations shall be based on the Watertown Square Design Handbook and such guidelines as the Planning Board may adopt.
- (h) **Signs**
 - (1) Any and all signage changes shall be subject to the design review process of Section 7.02 of the Zoning Ordinance.
 - (2) In reviewing all proposed signage changes within the Design Overlay District, the Planning Staff's recommendations shall be based on the Watertown Square Design Handbook and such guidelines as the Planning Board may adopt.
 - (3) All signage within the Design Overlay District shall conform to the provisions of Section 7.02 except as otherwise specified in the Watertown Square Design Handbook.

SECTION 5.10 LIMITED REDEVELOPMENT DISTRICT (RD)

- (a) **Intent and Purpose:** To encourage the redevelopment of land for retail purposes in order to promote the revitalization of land and to conserve its value.
- (b) **Permitted Uses:** The RD - Limited Redevelopment District shall be considered as overlaying other districts and is intended to permit more extensive uses than are permitted in the underlying zoning districts or in any applicable overlay district as follows: Retail establishments with more than five thousand (5,000) square feet of Building Area are permitted as of right.

Any use permitted by right or special permit in any of the underlying zoning districts in accordance with the provisions of Section 5.01 through 5.04 or in any overlay district applicable to the property located in the RD - Limited Redevelopment District, as permitted in such district shall not be affected by virtue of the provisions of this Section 5.11(b).
- (c) **Dimensional Criteria:** The uses noted in subsection (b) above shall be subject to the dimensional requirements applicable in the underlying zoning districts.
- (d) **Parking Requirements:** The parking requirements for development within the RD - Limited Redevelopment District shall conform to the parking requirements for each individual use set forth in Section 6.01 of this Zoning Ordinance.

- (e) **Signs:** For retail establishments permitted under Section 5.11(b)(i), the following signs shall be permitted: one wall sign for each business located in the building and in the case of a business which has more than a single department, one sign for each department provided such department occupies no less than 550 square feet of the building and provided the aggregate of all such wall signs on the building shall not exceed 500 square feet. For other uses, the provisions governing signs in the underlying zoning district shall be applicable.

SECTION 5.11 ASSISTED LIVING OVERLAY DISTRICT (ALOD)

- (a) **Intent and Purpose:** To encourage and guide the redevelopment of land for residential purposes and to promote its revitalization and value to the community.
- (b) **Permitted Uses:** Any use permitted in the underlying zone and Assisted Living as defined in Section 2.05, subject to special permit provisions under Section 9.04, 9.05, and 9.09.
- (c) **Dimensional Criteria:**

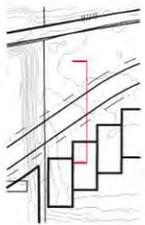
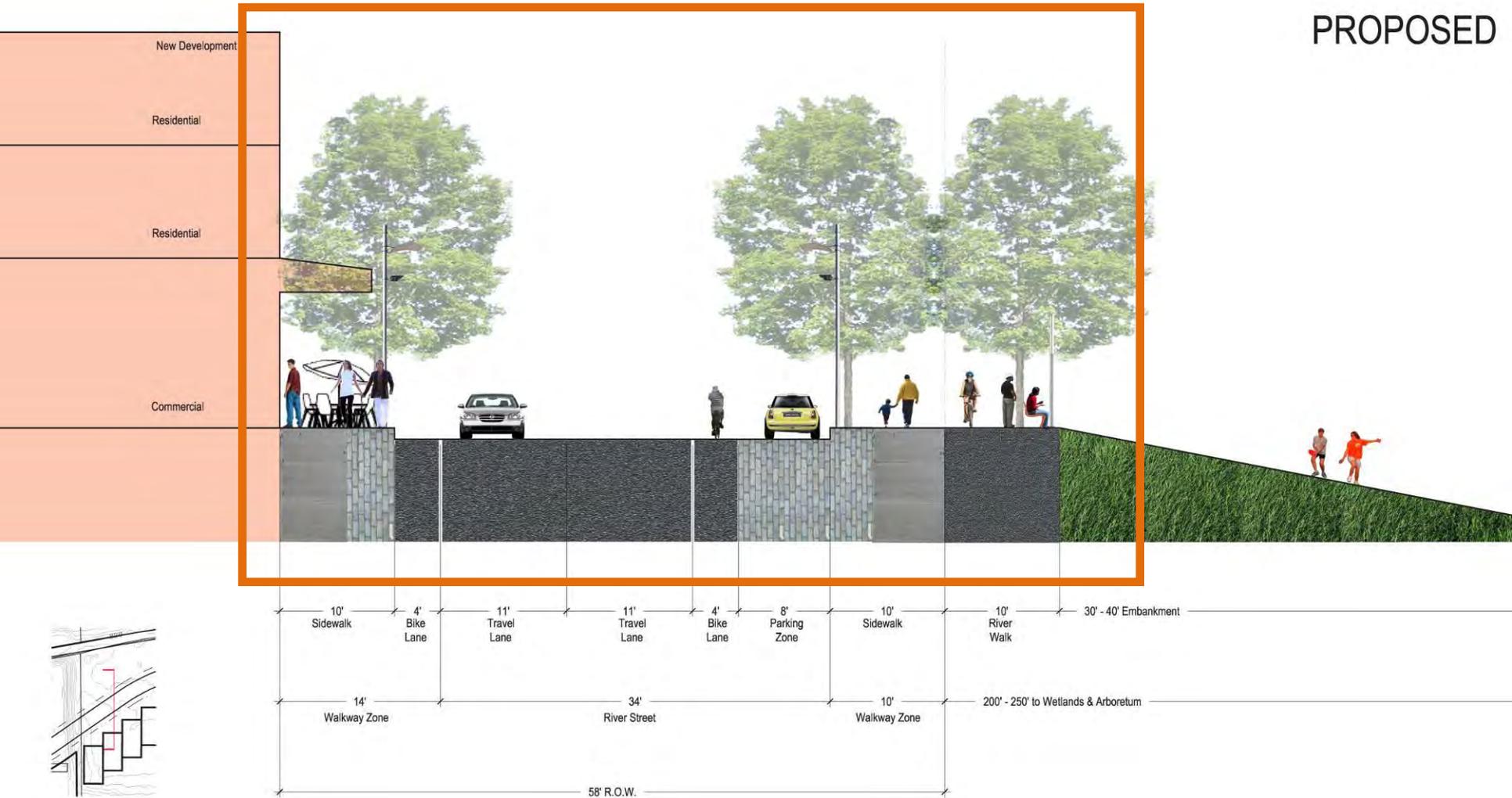
Min.Lot Size:	10,000 sf	Max.Building Coverage:	35%
Min.Frontage:	80 ft	Max.Height:	3 stories or the height allowed in the underlying district, whichever is greater, provided however that more than 3 stories may be allowed within existing structure.
Front Setback:	25 ft	Min.Lot/unit:	1,200 sf
Side Setback:	20 ft	Max.FAR:	1.0
Rear Setback:	20 ft	Min.Open Space:	20%
- (d) **Parking Requirement:** For Assisted Living, one space per every 4 units plus one per employee on largest shift. All other uses must follow the provisions of Section 6 of this Zoning Ordinance.
- (e) **Signage:** The provisions of Section 7.05 of this Zoning Ordinance shall apply.
- (f) **Affordable Housing Requirements:** A total of 10% of the units must be set aside as affordable pursuant to the provisions of Section 5.07 of this Zoning Ordinance.

SECTION 5.12 ARSENAL OVERLAY DEVELOPMENT DISTRICT (AODD)

- (a) **Name:** Arsenal Overlay Development District (AODD)
- (b) **Intent and Purpose:** To create a zoning overlay district to assist, promote, and guide the orderly conversion and redevelopment of former US Army Materials Technology Laboratory (AMTL) property in a manner which is beneficial to Watertown.
- (c) **Objectives:** The establishment of the AODD overlay district is intended to accomplish the following objectives:
 - (1) To permit the expeditious and efficient redevelopment of the AMTL property which will enhance the community's quality of life and economic well-being;
 - (2) To provide for compatibility of land uses within and abutting property;
 - (3) To promote the preservation of the historic resources and character of the property, including the rehabilitation of existing structures to the maximum extent practicable;
 - (4) To allow for a density which is appropriate for economic revitalization while not burdening the

Encourage greater emphasis on the **public realm**

PROPOSED



KNOXVILLE FORM BASED CODE: Source: Chan Krieger

WATERTOWN DESIGN GUIDELINES



WATERTOWN is an increasingly attractive development alternative

- Quality of life
- Neighborhood character
- Public transit
- Open space amenities
- Proximity to Cambridge, Boston, etc.

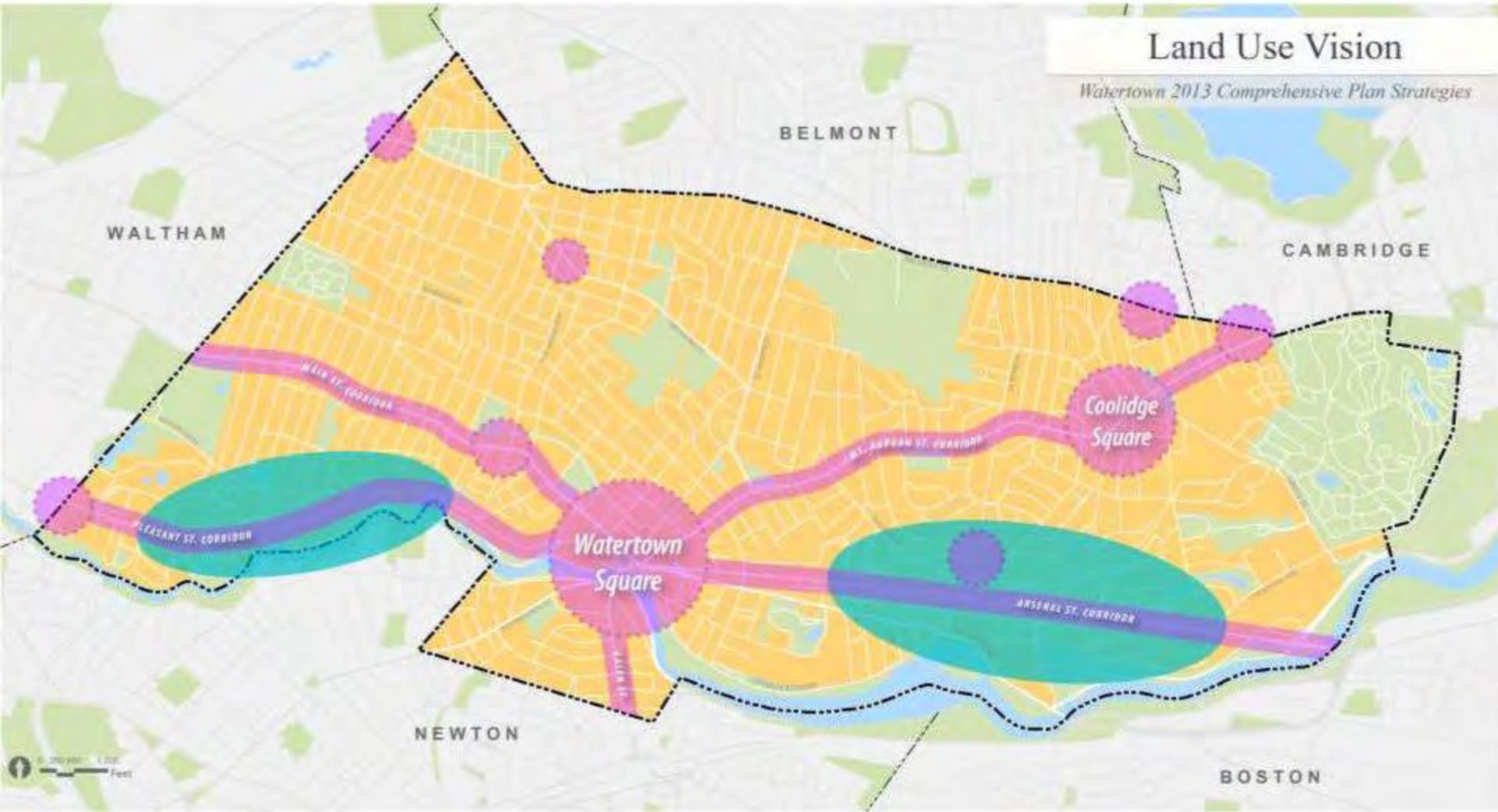
Single family home on Phillips Street



View looking east on N. Beacon Street

Land Use Vision

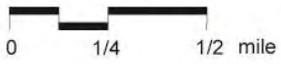
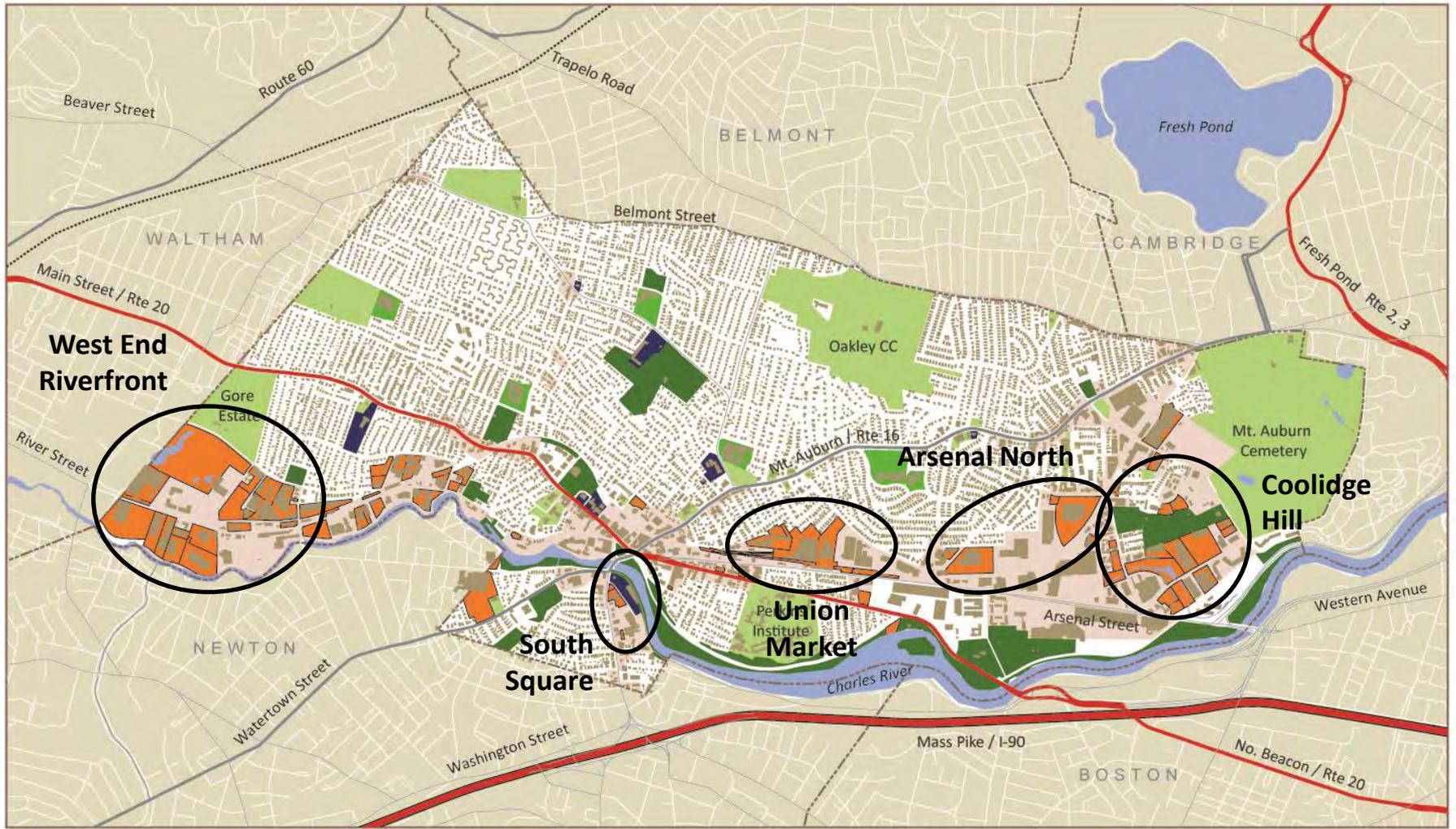
Watertown 2013 Comprehensive Plan Strategies



Land Use Vision

-  Areas to **Preserve**
-  Areas to **Enhance**
-  Areas to **Transform**

Source: VHB June 18, 2014



 Commercial/Industrial Properties with Assessments $< 30/sf$

Source: Mt. Auburn Associates / Madden Planning Group / Gamble Associates

3 What the Plan says

- ✓ **Vision Statement**
- ✓ **Priority (short-term) Implementation Actions**



Key Implementation Actions Land Use

Goal 1: Address design issues along the major corridors and Watertown and Coolidge Squares that enhances aesthetics while preserving neighborhood character.

- A. Enhance building, parking, and streetscape aesthetics by adding design and landscaping standards to the Limited and Central Commercial district regulations.
- B. Incorporate 'green' building practices to encourage energy efficiency and that projects are planned, designed, constructed, and managed to minimize adverse environmental impacts.

Source: VHB June 18. 2014



Enhancing Watertown Square as a Vibrant Town Center

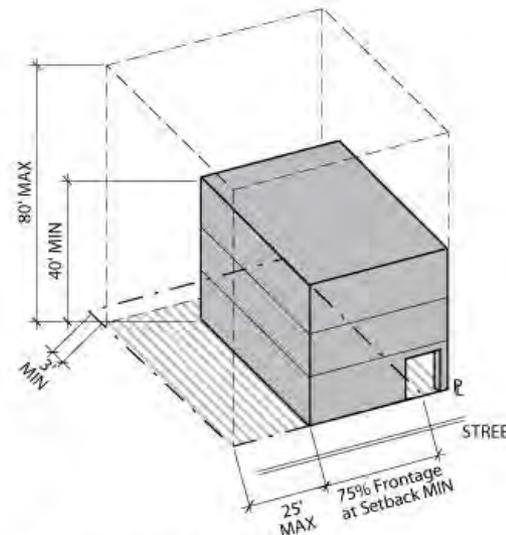
Watertown 2013 Comprehensive Plan Strategies



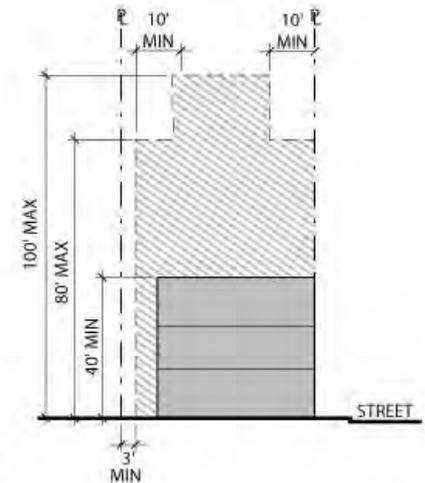
- 1 Reinforce Watertown Square as a Town Center
- 2 Strengthen the street edges to encourage walkability and pedestrian activities
- 3 Make the streets in Watertown Square more connected through advancing Complete Street strategies
- 4 Enhance the sidewalks (walkability/ livability)
- 5 Study Watertown Square to identify transportation enhancements that are possible to enhance traffic, safety, and operations
- 6 Improve pedestrian links to the river
- 7 Study parking needs and unlock the development potential for Watertown Square
- 8 Understand and advance infill development opportunities to make Watertown Square a more cohesive district of blocks and streets
- 9 Design and build a multipurpose trail system
- 10 Enhance the existing river walk trail and programming of activities such as kayak and canoe rentals
- 11 Make connections and streetscape improvements on the south side of the river and explore opportunities for the T station for TOD and/or multimodal enhancements

Source: VHB June 18, 2014

1. Improve the **character** of new development
2. Articulate standards of **quality**
3. Provide **examples**
4. Represent spatial and dimensional criteria **graphically**
5. Bring **clarity** to the review process
6. Go **too far!**



Axonometric Diagram



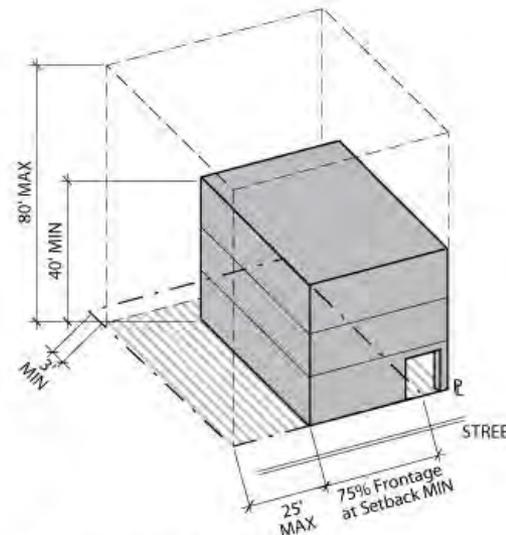
Section Height and Siting

GUIDELINES GONE TOO FAR: Seaside, FL

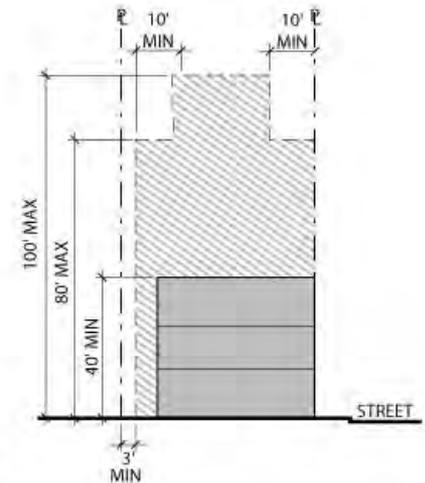


1. Regulate **building use**
2. **Replace** zoning or building codes
3. Redesign streets and **public right-of-way**
4. **Master plan** areas of the Town.

Design Guidelines are only as strong as the mechanism to enforce them.



Axonometric Diagram



Section Height and Siting

Past Injustices





Problem Areas



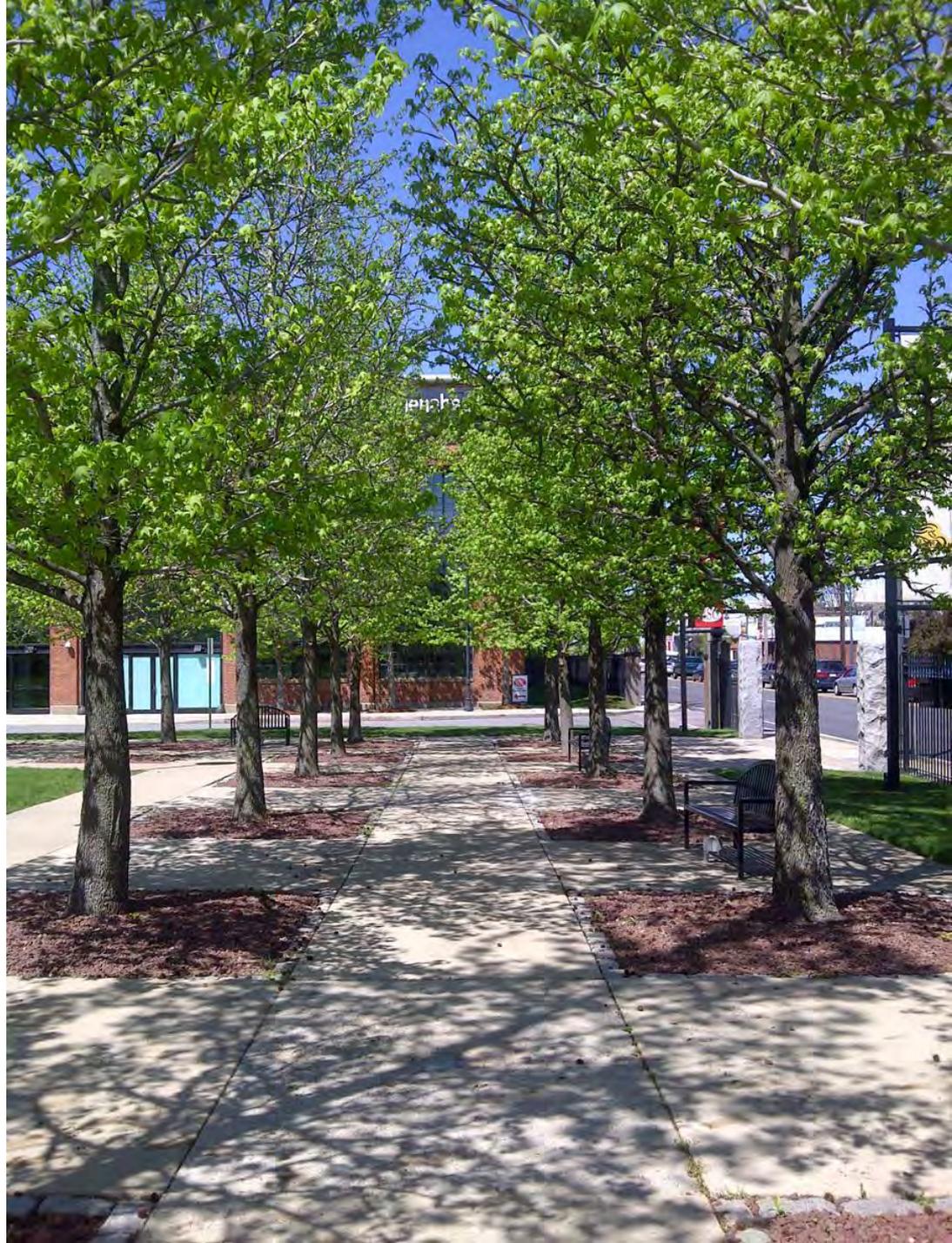
Problem Areas



Parking Buffers



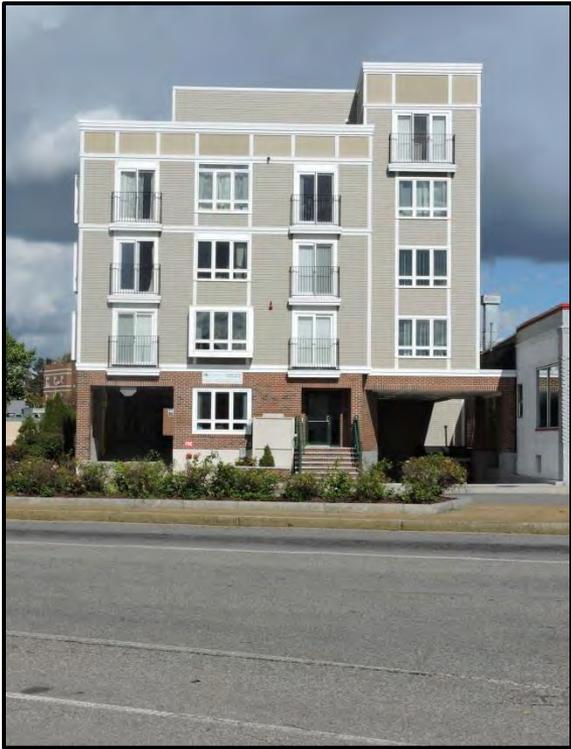
Landscape treatment





Past Injustices

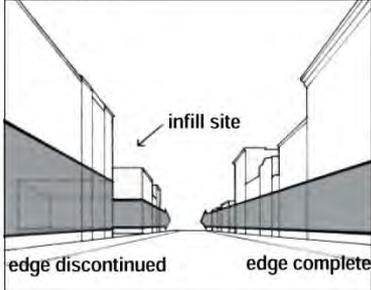
RECENT CONSTRUCTION



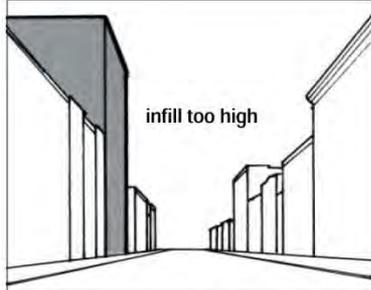
Minneapolis Third Ward

FORM

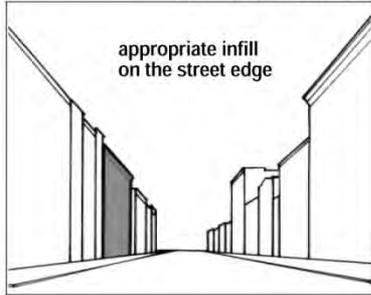
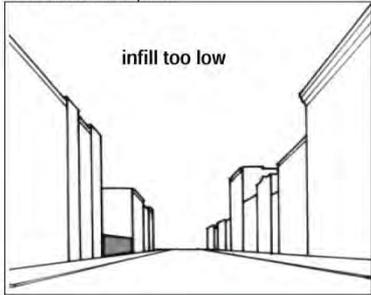
infill sites



Existing Vacant Sites
Undesirable Development



Undesirable Development
Desirable Infill of Vacant Site



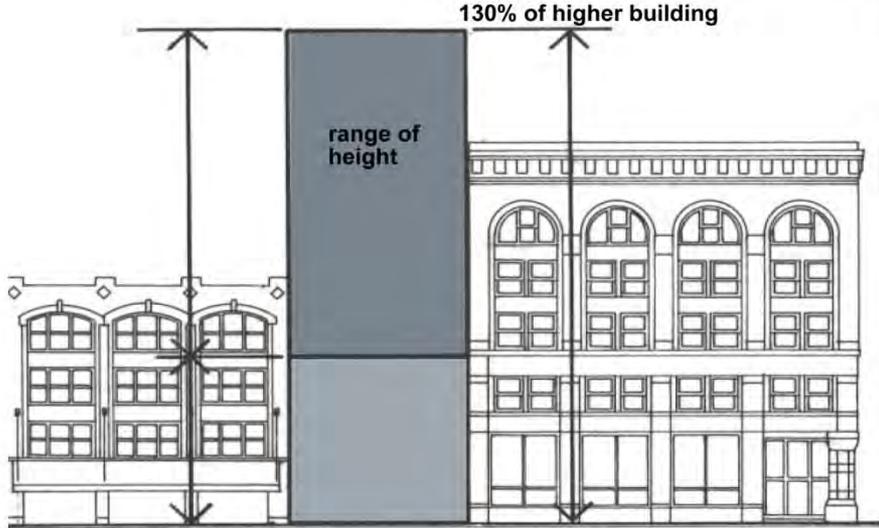
Design Guideline: Street edge continuity shall be re-established by infill development of vacant sites.

Design Principle: Missing building fronts along a blockface disrupts the quality of street as a place by allowing the enclosed space to leak out.

The urban quality of the District has much to do with the nature of the street. The key to this is the notion that building facades contain space. This is in contrast to the suburban or rural pattern of landscapes containing buildings.

Where buildings once stood within the Historic District, a number of vacant sites have been created. To re-establish the quality of the street these sites should be filled with new buildings of similar height and scale.

facade height



70% of lower building but not less than 30 feet

Height of New Facades

Design Guideline: The height of buildings within the Historic District shall be contained within the range of 70% to 130% of adjacent historic building heights along the street edge, but not less than three stories (30 feet).

Design Guideline: The height of buildings outside the Historic District shall comply with the guidelines on page 14.

Design Principle: Strong neighborhood identity is reinforced by a commonality of building forms referred to as an urban fabric.

Design Principle: "On the whole, adjacent buildings should be roughly the same height."

The height of buildings in the Neighborhood varies widely from one story to ten stories. By relating the indicated percentages to the historic buildings a general continuity of height at the street edge is maintained. This will preserve the perceived scale of the buildings and extend the harmony of street spaces that exist within the Historic District. **Buildings extending through to adjacent streets shall conform on each street.**

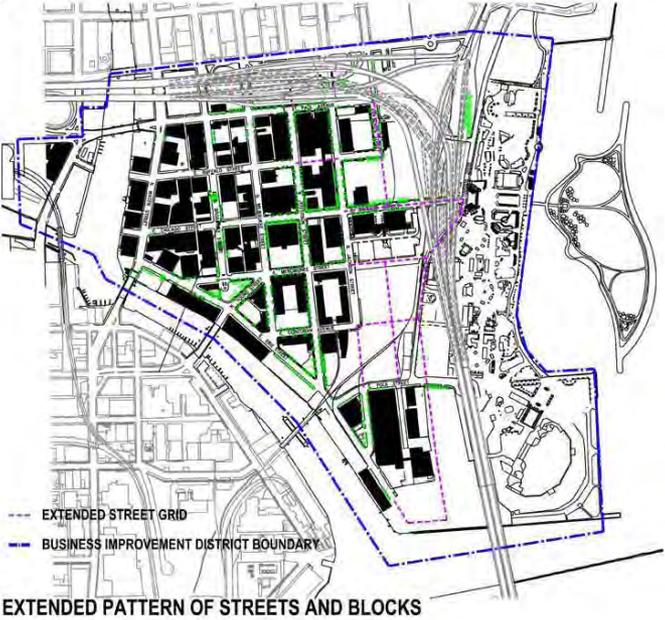
Historic District Corner Sites: For sites on intersecting rights of way of different widths the ARB will entertain a greater building height up to the lesser adjacent street width if of exceptional design quality and if building massing transitions to lower existing structures.

Exception: See page 37, Landmark Sites
* Excerpt from *A Pattern Language*, by Christopher Alexander.

Minneapolis Third Ward

SPATIAL ORGANIZATION OF THE DISTRICT

pattern of streets and blocks



EXTENDED PATTERN OF STREETS AND BLOCKS

Design Guideline: In general, the existing pattern of streets and blocks should be maintained and extended

Design Principle: Street layout fundamentally guides the scale of future development and can ascribe cultural significances by variation within the pattern of streets and blocks.

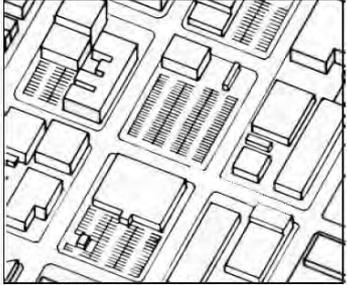
Historically, the scale of development within the District has been constrained by the size of a city block and available building technologies. The railway coachyards, however, were never subdivided into city blocks.

Recent development practices have permitted larger buildings that either block existing streets or occupy an area larger than one city block.

Two rejected proposed developments, the Italian Community Center scheme by Bonifica and the Baseball Stadium proposal, were multi-block developments of a scale that would have dramatically contrasted with the scale of the Historic Third Ward. In order to consistently extend the scale of the Historic District, existing streets should be maintained and no further encroachment of buildings into streets should be permitted.

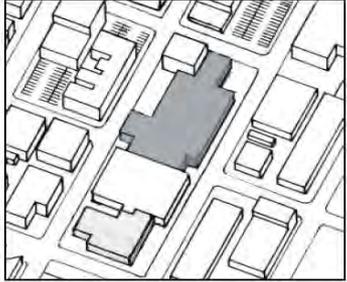
New streets should expand the city grid to the east up to the freeway to create a pattern of streets and blocks that is generally consistent with the historic urban fabric. This is a fundamental step in guiding the scale of future development.

development pattern



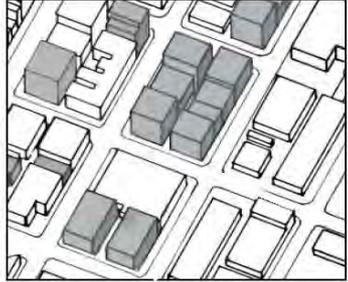
Existing Surface Parking

Undesirable Low Rise Development



Undesirable High-Rise Development

Desirable Pattern of Mid-Rise Development



Design Guideline: As development proceeds, vacant sites should be filled with mid-rise buildings, including parking structures.

Design Principle: The height of new buildings should not vary too much from the predominant height of buildings in the Historic District.

Typically the pattern of building within the District has been mid-rise structures occupying the majority of the city block. Urban attrition has resulted in major gaps within the urban fabric. These gaps have afforded the opportunity to inject needed surface parking lots into the area. New industrial building resulting from changing transportation patterns and freeway access to the area cleared out existing development and established a pattern of large single level industrial buildings with adjacent surface parking areas.

The decline of the rail network further added to the attrition and created the large vacant area of the Coachyards (Map, p 10).

This suburban industrial park land use pattern and resultant building type is not the highest and best use, and is inappropriate in this urban location and out of character with the evolving neighborhood.

Just as low single-story development must be avoided so must the prevalent urban pattern of high-rise development with surrounding surface parking. Where possible parking should be concentrated in multilevel structures to fill voids along streets in the neighborhood

Coolidge Corner Overlay District, Brookline MA

DESIGN GUIDELINES FOR COOLIDGE CORNER INTERIM PLANNING OVERLAY DISTRICT

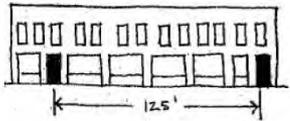
2.4 Spacing of residential and commercial entrances

Purpose: Encourage active streetscapes with multiple entrances; discourage long stretches of “dead” space along the street.

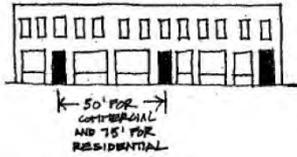
Guideline: Where residential buildings face a public street, entrances should generally be spaced no more than 75 feet apart. Where commercial storefronts face the street, doors should generally be spaced no more than 50 feet apart.



Harvard Street, Coolidge Corner
CVS Pharmacy in Coolidge Corner has two entrances along Harvard Street that are spaced approximately 70 feet apart. An additional entrance was required by the Planning Board when the storefront was expanded.



Discouraged



Preferred

DESIGN GUIDELINES FOR COOLIDGE CORNER INTERIM PLANNING OVERLAY DISTRICT

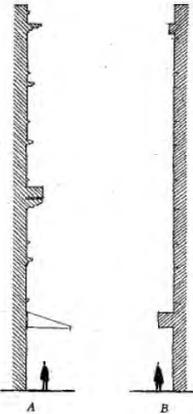
3.7 Surface articulation

Purpose: Encourage richness in building surface design to ensure that new buildings and additions contribute to the public realm.

Guideline: The facades of buildings should be designed to include surface variation that maximizes the interplay of light and shadow.



These photos illustrate extensive surface variation, providing a sense of quality and permanence.



Building wall sections of two buildings on Via Cola di Rienzo, Rome; the more complex facade, A, offers more surfaces, more opportunities for shadow and light changes than does facade B

Source: *Great Streets*, Allan Jacobs

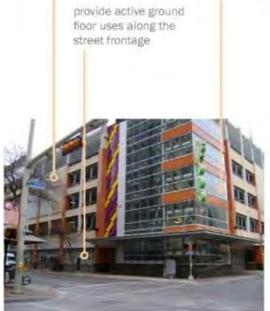
San Antonio, TX Downtown Design Guideline

BEST PRACTICES Downtown San Antonio

PARKING STRUCTURE: ARCHITECTURAL TREATMENT

external skin covers basic concrete structure of ramps, walls, and columns

elevator and stairs are located on the primary pedestrian corner and highlighted architecturally



GROUND FLOOR COMMERCIAL: ARCHITECTURAL TREATMENT

wall openings comprise at least 70% of the facade

integrated canopy defines and adds comfort to the pedestrian space



ON-SITE OPEN SPACE

located on the ground level and inviting to the general public

lined with ground floor restaurants or retail



HORIZONTAL VARIATION



details and materials are varied horizontally to provide scale and three dimensional qualities



VERTICAL VARIATION



building was designed with towers of varying heights to break-up the vertical mass and differentiate lower floor from upper floor functions



INFILL BUILDINGS

new development reflects historic relationship to street, 1st floor plate height, & window configuration

transom windows

historic projecting blade sign

bays break-up horizontal mass



Chapter 2 SIDEWALKS AND SETBACKS

A. SIDEWALKS

The City of San Antonio has the opportunity to reinforce downtown as a distinct living and entertaining environment: a pedestrian precinct, where pedestrians share the downtown with automobiles and public transportation. The mix of traffic can provide a sense of excitement and actually enhance the pedestrian's experience if these other elements are kept in balance. Paramount could be providing a sense of comfort for pedestrians. This includes ensuring that sidewalks are designed to facilitate walking and that public spaces are created which are lively and inviting.

Design sidewalks that are walkable and accommodate a variety of uses.

1. Provide a minimum 72 inch wide continuous pedestrian path of travel as seen in Figure 2.1.
2. Provide an 18 inch to 24 inch wide access or utility zone next to the curb, which includes the six (6) inch curb and 12 inch wide decorative granite or brick edge band adjacent to the back of curb.
3. Outdoor dining may occur on any portion of the paved sidewalk provided a minimum wide (72 inches) continuous clear path of public travel is maintained and obtain proper permits

Design sidewalks to accommodate and support large street trees and collect storm water, and allow for continuous parkways where appropriate.

4. Provide continuous landscaped and hardscaped area, commonly referred to as "parkway," adjacent to the curb on predominantly non-commercial streets. The continuous landscaped and hardscaped parkways should be designed to collect and retain or treat storm runoff.
5. In an ideal urban tree canopy, adjacent trees at street maturity generally touch one another. Therefore, typical tree spacing is generally 30 to 50 feet apart, depending upon the tree species.
6. Plant or replant street trees to shade and shelter the pedestrian from sun, rain and traffic, and to improve the quality of the air and storm water runoff.

Where it is not feasible to plant street trees in continuous landscaped parkways, (i.e. Ultra Urban Streets such as Commerce Street) provide large street wells with gap-graded soil beneath the sidewalk.

7. Trees shall be planted in tree wells within tree grates that are at least 5 feet long and a minimum of 5' feet wide.
8. Where tree wells and parkways would conflict with existing basements, underground vaults, historic paving materials, or other existing features



Fig 2.1 Example showing the parkway along the curb, the clear path of travel and use of the remaining sidewalk for outdoor dining.

San Antonio, TX Downtown Design Guideline

- 2. Vertical pedestrian circulation cores (elevator and stairs) should be located on the primary pedestrian corners and be highlighted architecturally so visitors can easily find and access these entry points, in order to eliminate pedestrians using the vehicular ramps.
- 3. Parking structures should integrate sustainable design features such as photovoltaic panels (especially on the top parking deck), renewable materials with proven longevity, landscaping and storm water treatment wherever possible.



Fig 4.6 Parking Garage Without External Skin (Discouraged)

- 4. Treat the ground floor along active pedestrian oriented public streets as specified in Chapter 3: to provide active ground floor uses along the street frontage of the garage; on all other streets the ground floor treatment should provide a low screening element that blocks views of parked vehicle bumpers and headlights from pedestrians using the adjacent sidewalk. Additional treatments such as "live" green walls similar to a Chia Pet provides for a more aesthetic and pleasing façade.
- 5. Signage and way finding should be integrated with the architecture of the parking structure as seen in Fig 4.7.
- 6. Integrate the design of public art and lighting with the architecture of the structure to reinforce its unique identity. This is especially important

for public parking structures to aid in visitors finding them upon arrival and getting oriented to downtown Fig 4.8.

- 7. Interior garage lighting should not produce glaring sources towards adjacent residential units while providing safe and adequate lighting levels per code.



Fig 4.7 Parking Garage Wayfinding Concept



Fig 4.8 Parking Garage Identity (Encouraged)



8 Story Stacked Flats with Town Houses
75 DU/AC

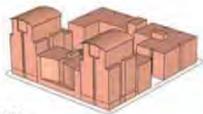


8 Story Stacked Flats
100 DU/AC



Fig 5.2 Mid Rise Residential Massing Diagrams

Mid-rise residential building types can be used to achieve higher density levels than low-rise, but require more expensive construction.



12-16 Story Stacked Flats
125 DU/AC



30 Story Stacked Flats
250 DU/AC



Fig 5.3 High Rise Residential Massing Diagrams

High-rise residential building types can be used to achieve very high density levels, and require Type I construction, which typically results in higher value units.

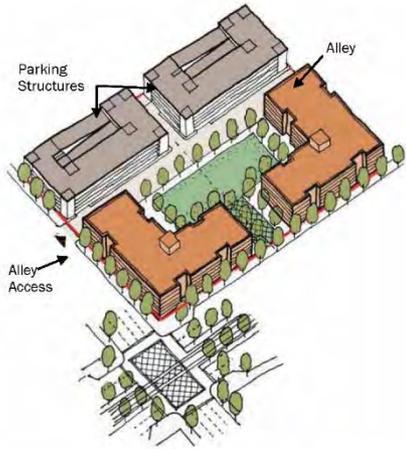


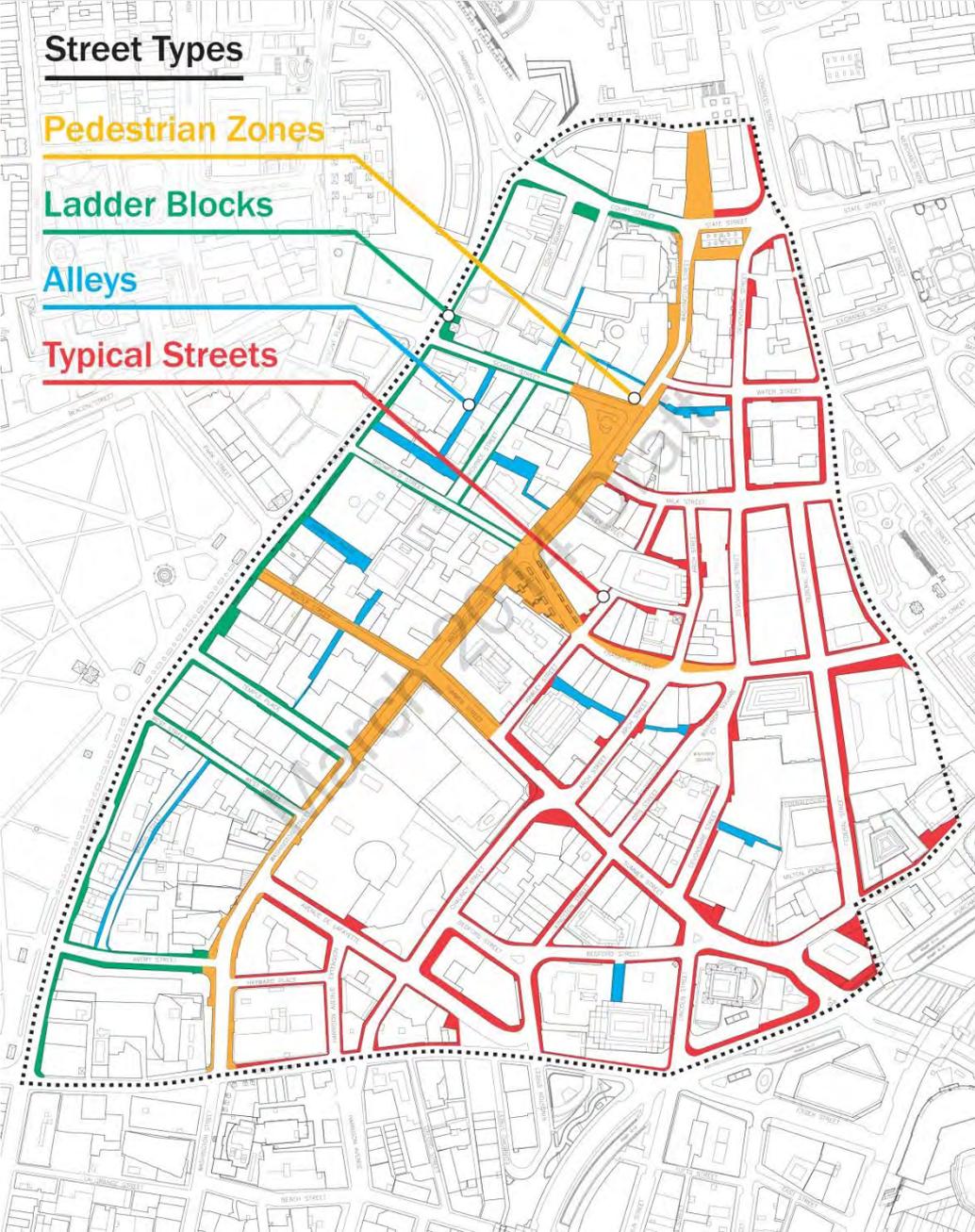
Fig 5.4 Low Rise Commercial Massing Diagrams

Low-rise commercial buildings should be placed along the property line, with little setback provided. Their massing should form open spaces. High parking ratios require structured parking often almost equivalent in gross square feet to the building space it serves.

DESIGN GUIDELINES

What are others doing?

Source: KMDG, HR+A / Gamble Associates



Downtown Crossing, Boston



Encourage



Where appropriate, encourage street trees

Street trees are essential elements in determining the character of an urban space. They play an important role in providing shade which combats urban heat island effect, filter urban air, and supports continuity of urban habitats for birds.



Consistent paving treatment

Creating a uniform paving treatment gives a complex space a consistent 'carpet' allowing the whole district to be understood as a place. The consistent pattern creates uniformity in contrast to the diversity of building façades.



Encourage porosity between interiors and exteriors.

Visual and/or physical porosity of facades supports a more lively streetscape. The use of operable facades and cafe seating enliven and diversifies the public realm. Non-food related businesses can also support dynamic street life by opening sight lines into facades.



Discourage



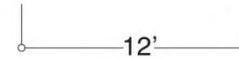
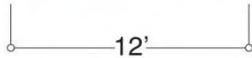
Avoid clashing materials

The juxtaposition of paving materials usually indicates modifications to the sidewalk or street over time. The goal is to eliminate all such places within the DBBID and make pavement more consistent throughout.

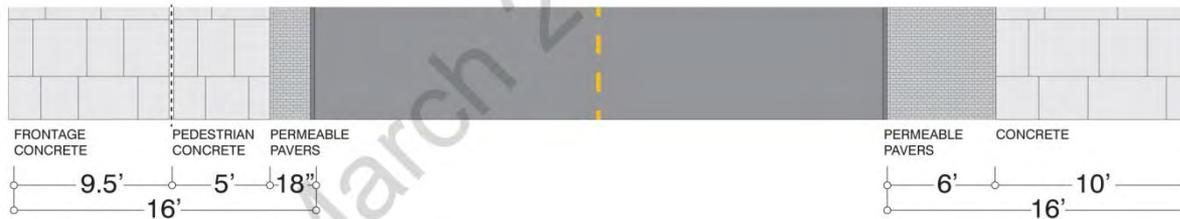


Avoid furniture outside of guideline approved

The branding and identity of the DBBID is reinforced by the physical elements of the streetscape. Utilization of furniture outlined in this guidelines maximizes district brand identity.



Downtown Crossing, Boston

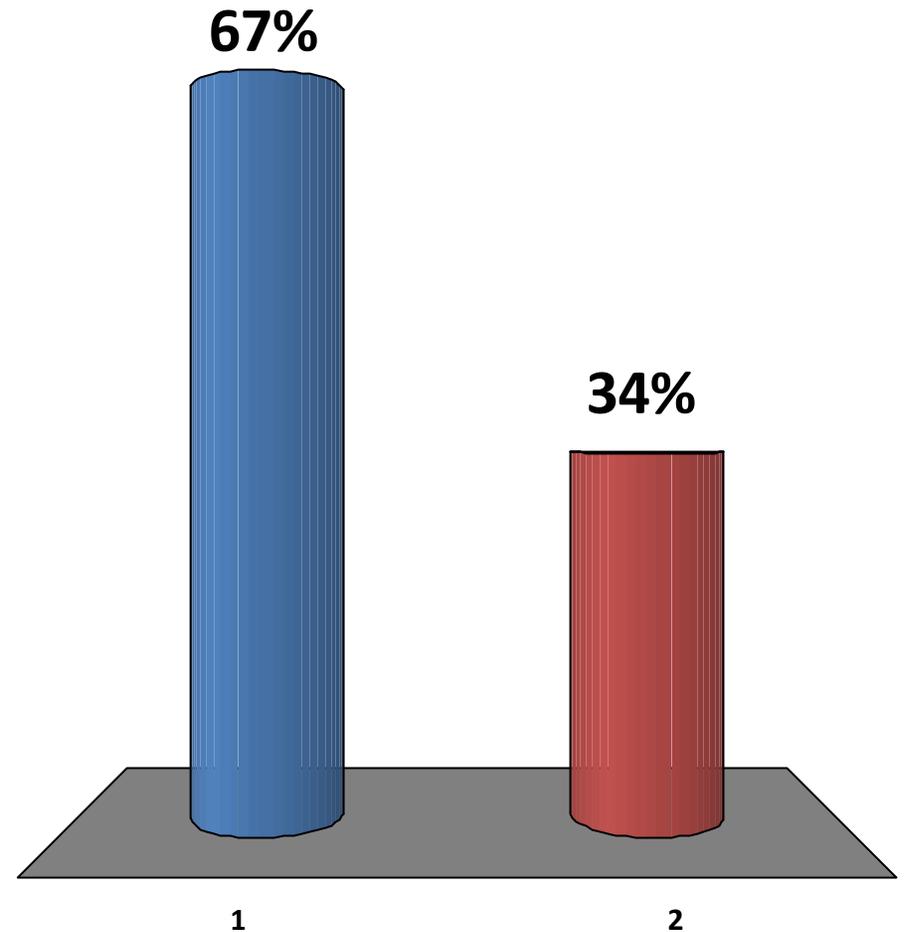


Downtown Crossing, Boston



I have used this method of engagement before

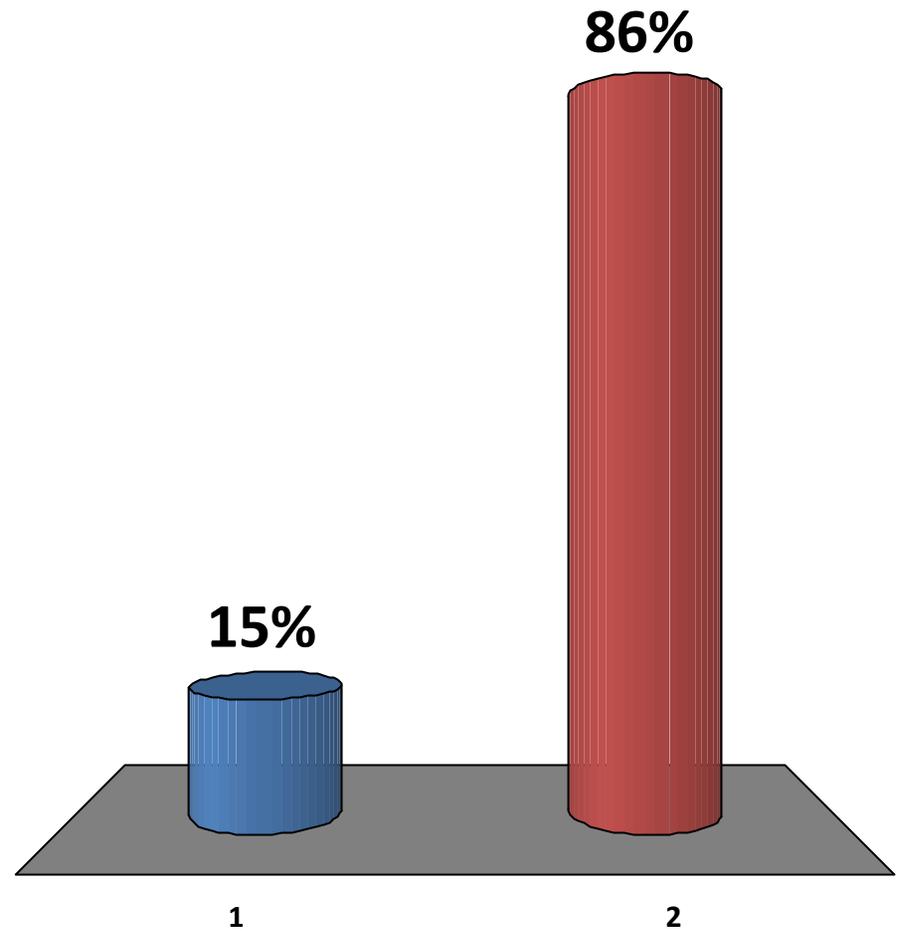
- 1. Yes
- 2. No



I was born in Watertown:

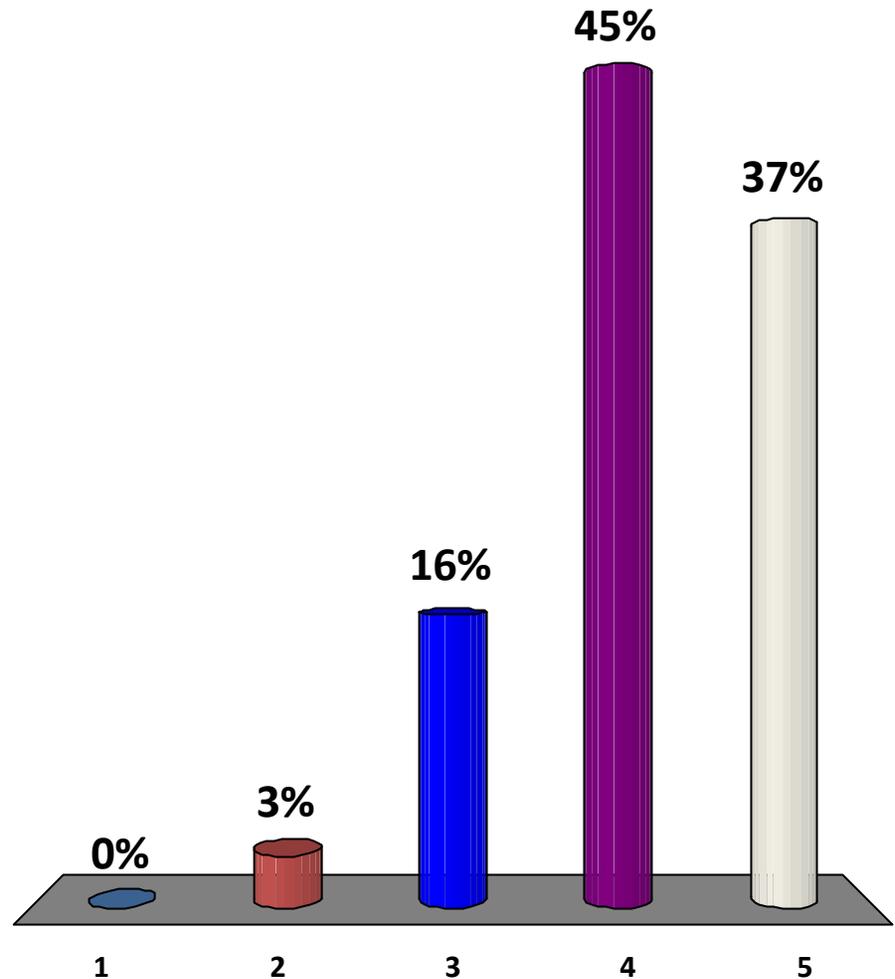
1. Yes

2. No



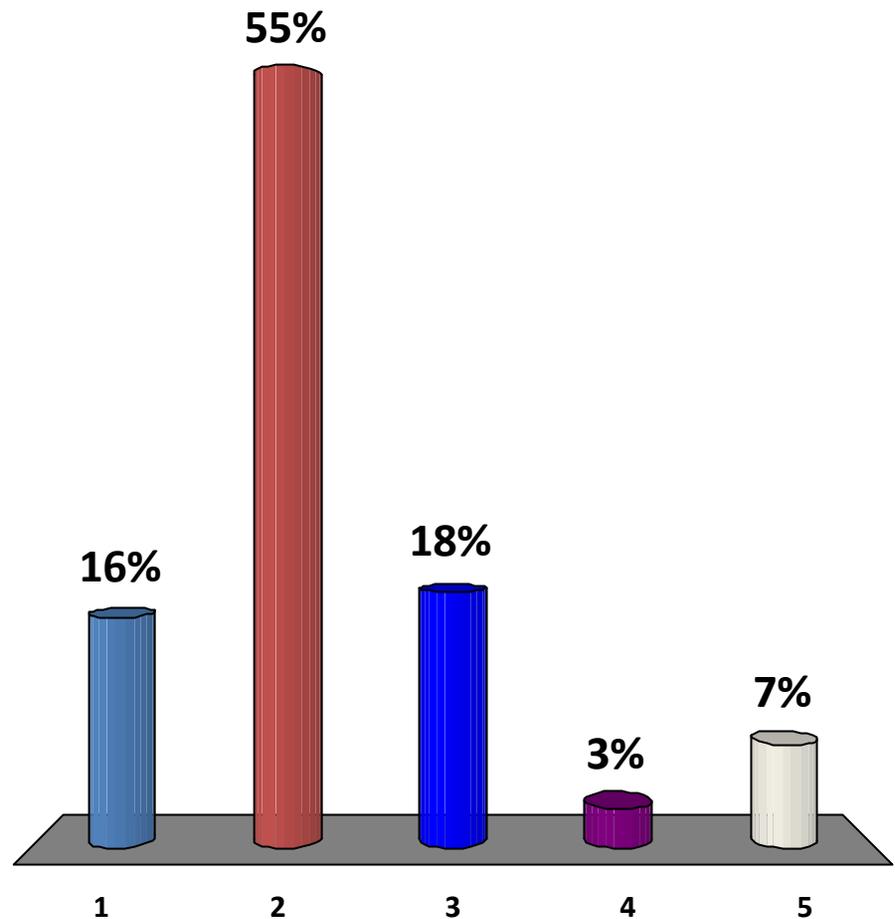
My age is:

1. Under 20
2. 21-34
3. 35-49
4. 50-65
5. 65+



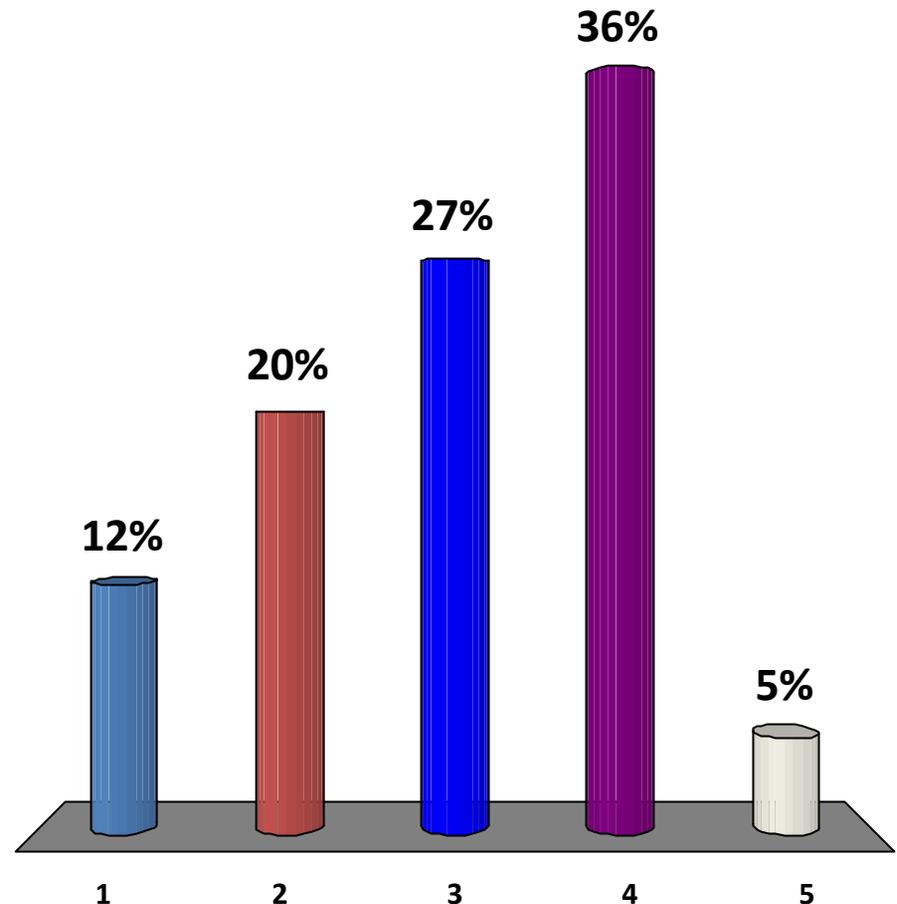
In live closest to:

1. Pleasant Street
2. Watertown Square
3. Coolidge Square
4. Mt. Auburn Cemetery
5. I do not live in Watertown



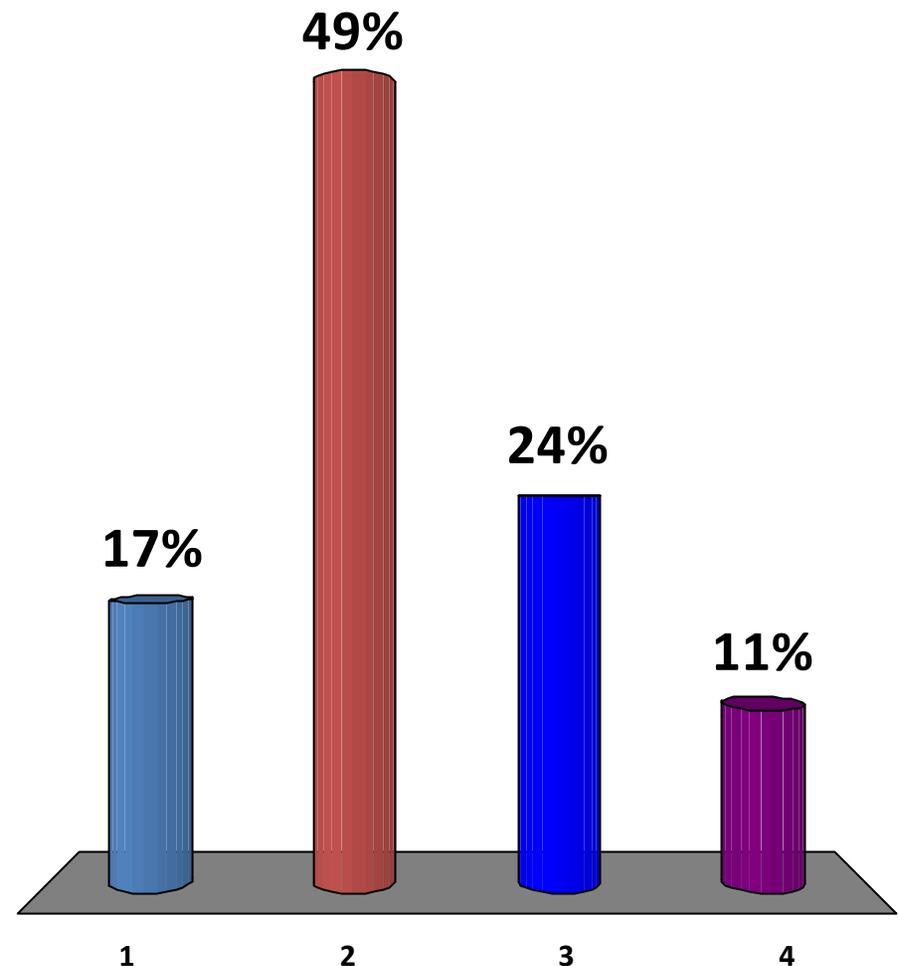
My biggest cause of anxiety right now is...

1. The amount of new development
2. The scale of new buildings
3. The quality of the private/public realm
4. The lack of a coordinated vision
5. Other



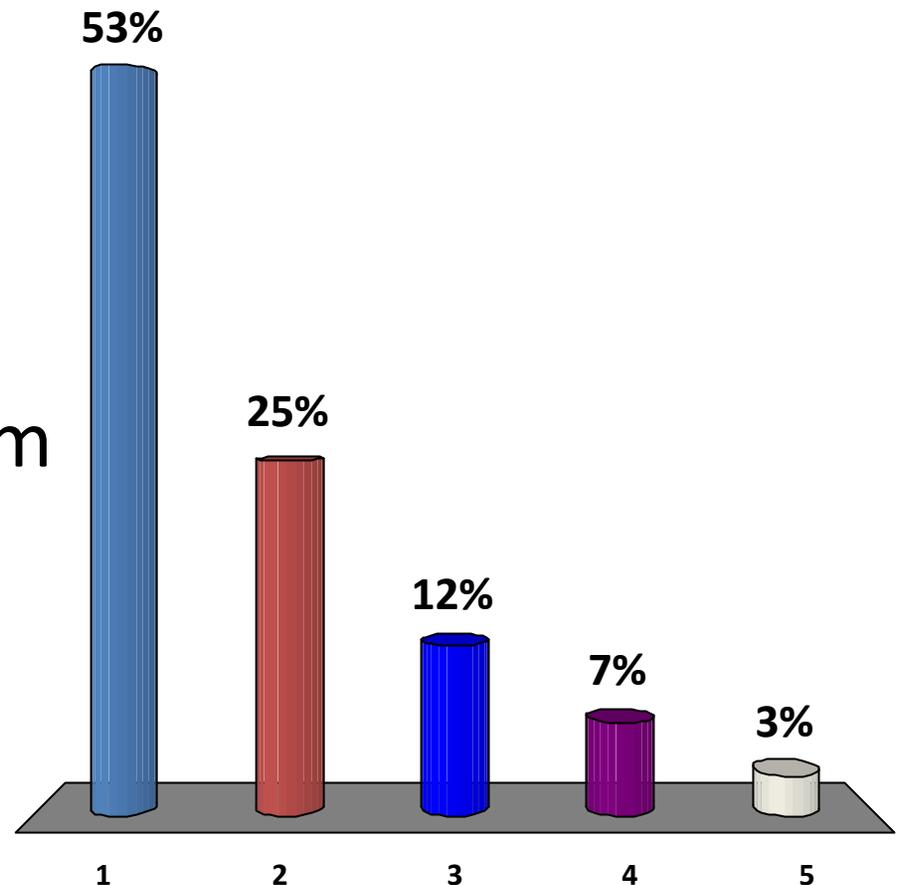
What is most important in terms of the character of the Town?

1. Preserving and beautifying open space
2. Integrating residential and commercial growth
3. Addressing congestion
4. Enhancing the commercial centers



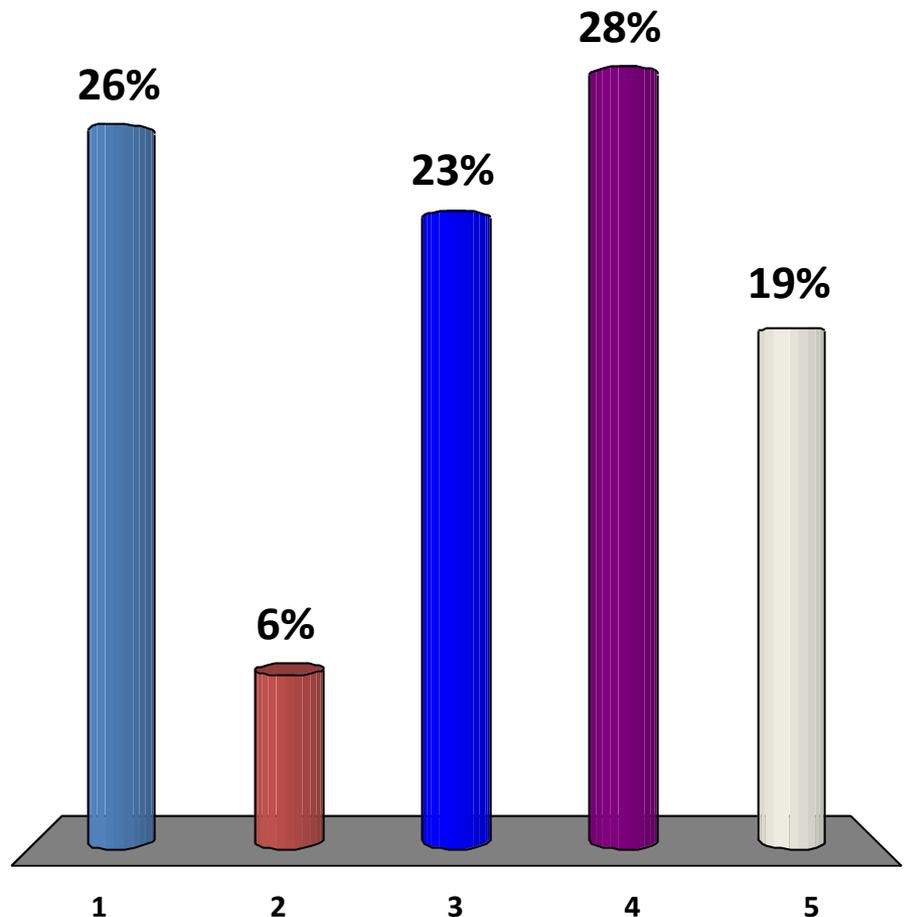
What are your expectations for developing Design Guidelines?

1. They will raise the quality of new development
2. Provide direction to developers
3. Remove ambiguity from the process
4. They will have little impact
5. Other



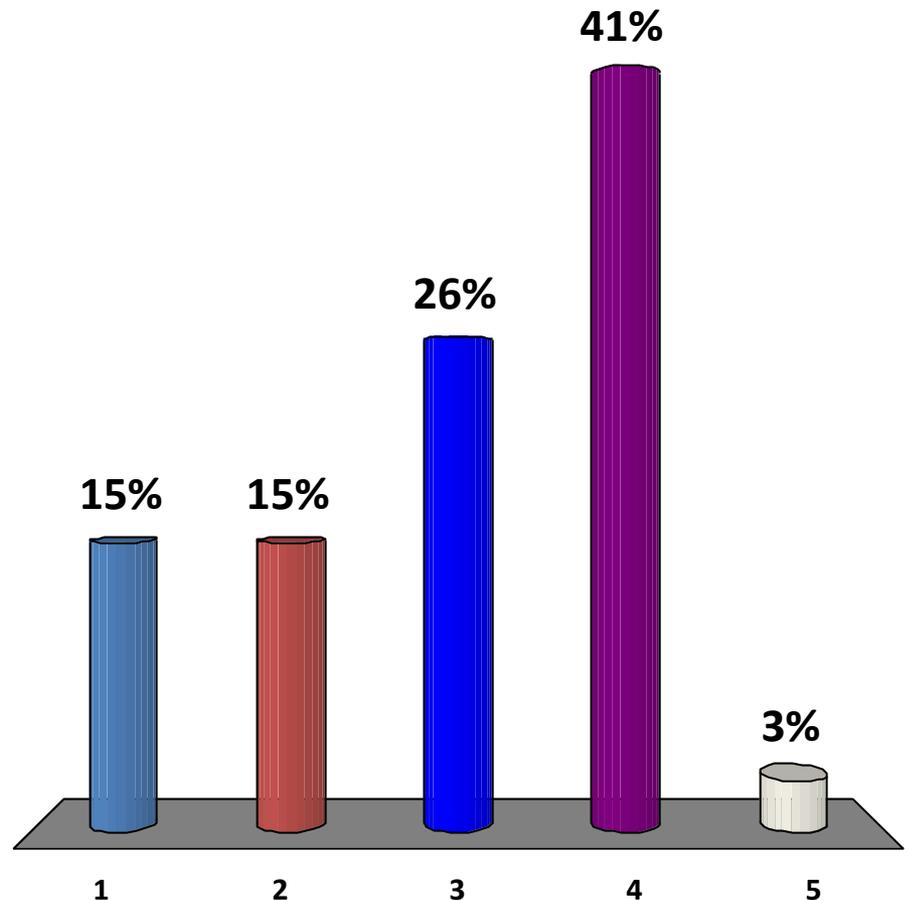
What issue is the most pressing in terms of development?

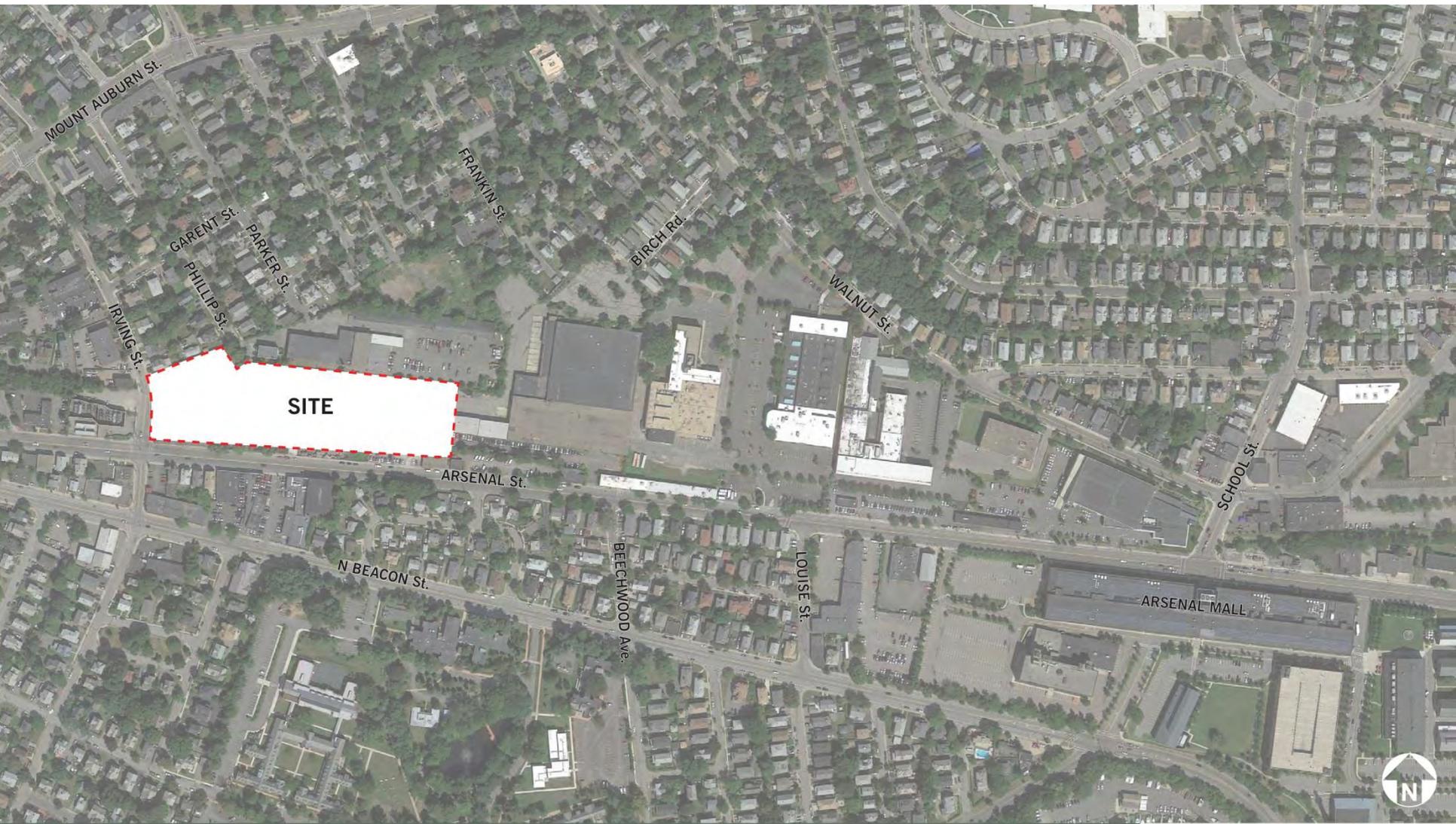
1. Scale of development
2. Location of development
3. Design aesthetic
4. Traffic, circulation and parking
5. Environmental



In terms of new development, what issue is the most important to get right?

1. Building heights
2. Cultivating a mix of uses
3. Site plan design, access and parking
4. Design quality
5. Other





SITE

Elan Watertown

Arsenal Street

Proposed Land Distribution

March 25, 2013

DEVELOPER : Greystar

ARCHITECT : The Architectural Team

tat | the architectural team

SITE PLAN REVIEW

Greystar

ELAN WATERTOWN

IRVING AND ARSENAL WATERTOWN, MA

SUBMISSIONS:

SITE PLAN REVIEW	MARCH 21, 2014
SITE PLAN REVIEW REVISIONS	JUNE 16, 2014



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 F 617.884.4329
 www.architecturalteam.com
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LOCAL PARTNER	OAKTREE FX, LLC 84 SHEPARD STREET CAMBRIDGE, MA 02140 PHONE #: 617.491.9100 FAX #:
ARCHITECT	THE ARCHITECTURAL TEAM, INC. 10 COMMANDANT'S WAY AT ADMIRAL'S HILL CHELSEA, MA 02150 PHONE #: 617.889.4402 FAX #: 617.884.4329
CIVIL ENGINEER	ALLEN & MAJOR ASSOCIATES, INC. 100 COMMERCE BLVD ROSLINDEN, MA 01969 PHONE #: 781.355.0669 FAX #: 781.355.2096

Consultant:

Revision:

1	2014.06.16	SITE PLAN REVS

Architect of Record:

Drawn: A.S.
 Checked: T.S.
 Scale:
 Key Plan:

Project Name:
ELAN WATERTOWN

IRVING AND ARSENAL WATERTOWN, MA

Sheet Name:
PROJECT COVER

Project Number:
13099

Issue Date:
MARCH 21, 2014

Sheet Number:
T0.01



Source: Greystar

8/6/2014 4:05:41 PM
 C:\Users\mwh\OneDrive\Architectural\Sheet\T001b.rvt

Source: Hanover Co.



ARSENAL STREET MIXED-USE DEVELOPMENT

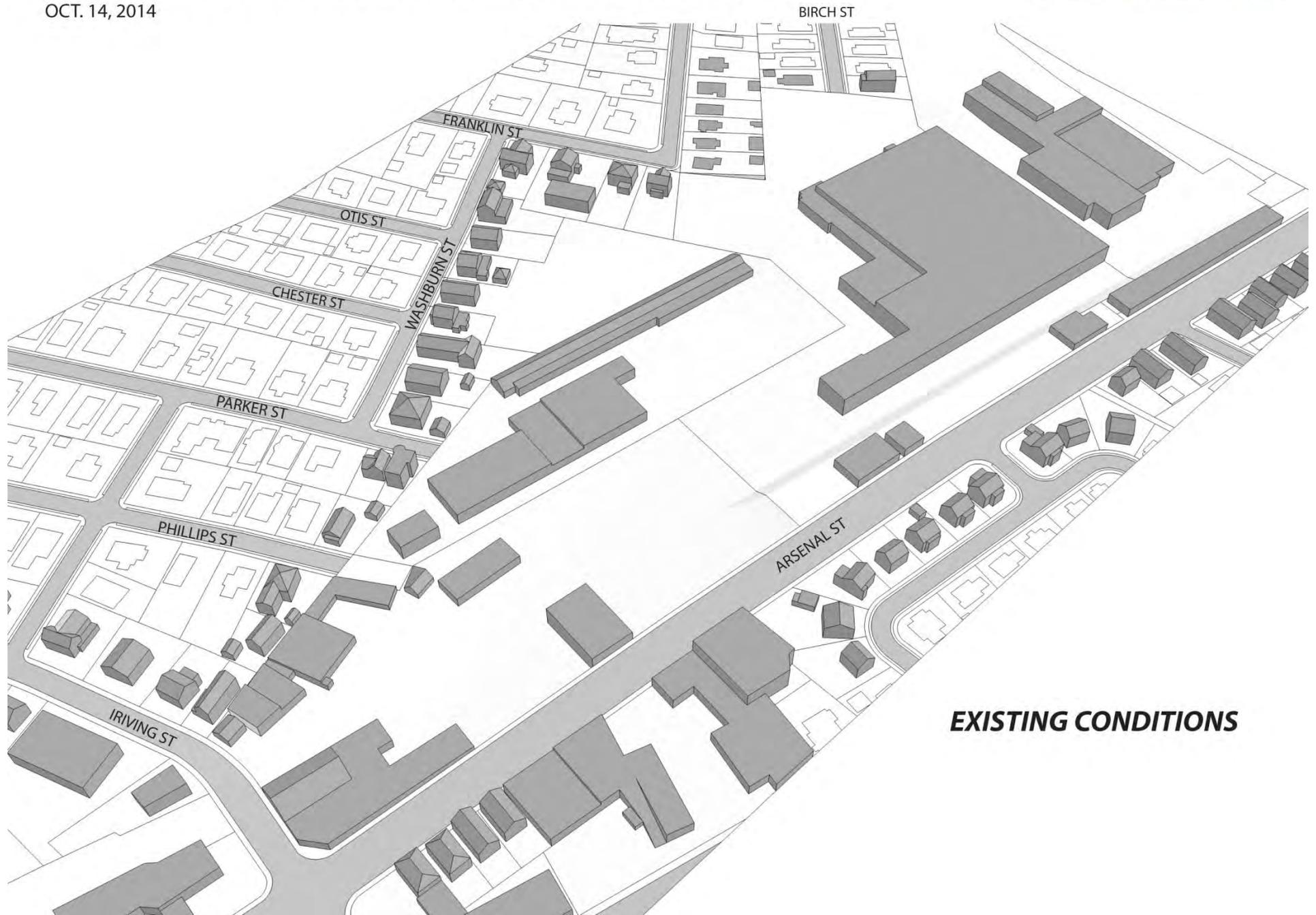
ZONING BOARD HEARING

June 25, 2014
Watertown, MA 02472



ARSENAL ST CORRIDOR STUDY (DRAFT)

OCT. 14, 2014



EXISTING CONDITIONS



Arsenal Street



Drive-Thru
Clearance 9 Feet

STOP

McDonald's
Drive-Thru

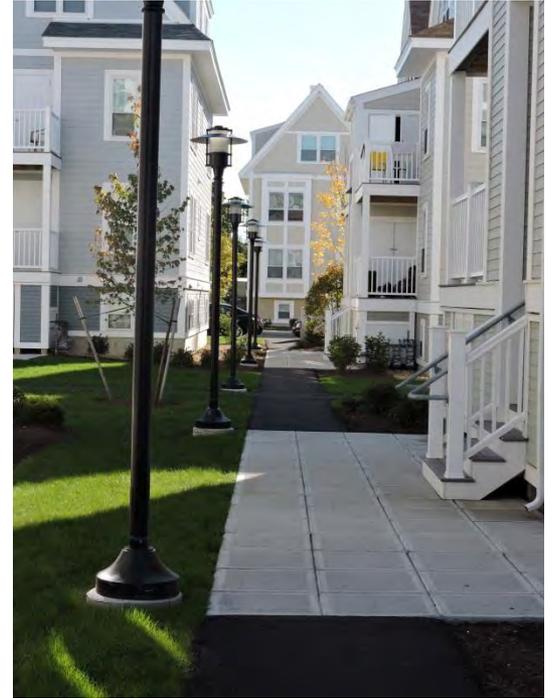
NO CONSTRUCTION
PARKING

U



Source: Archboston.org





MAXWELL GREEN

Somerville, MA











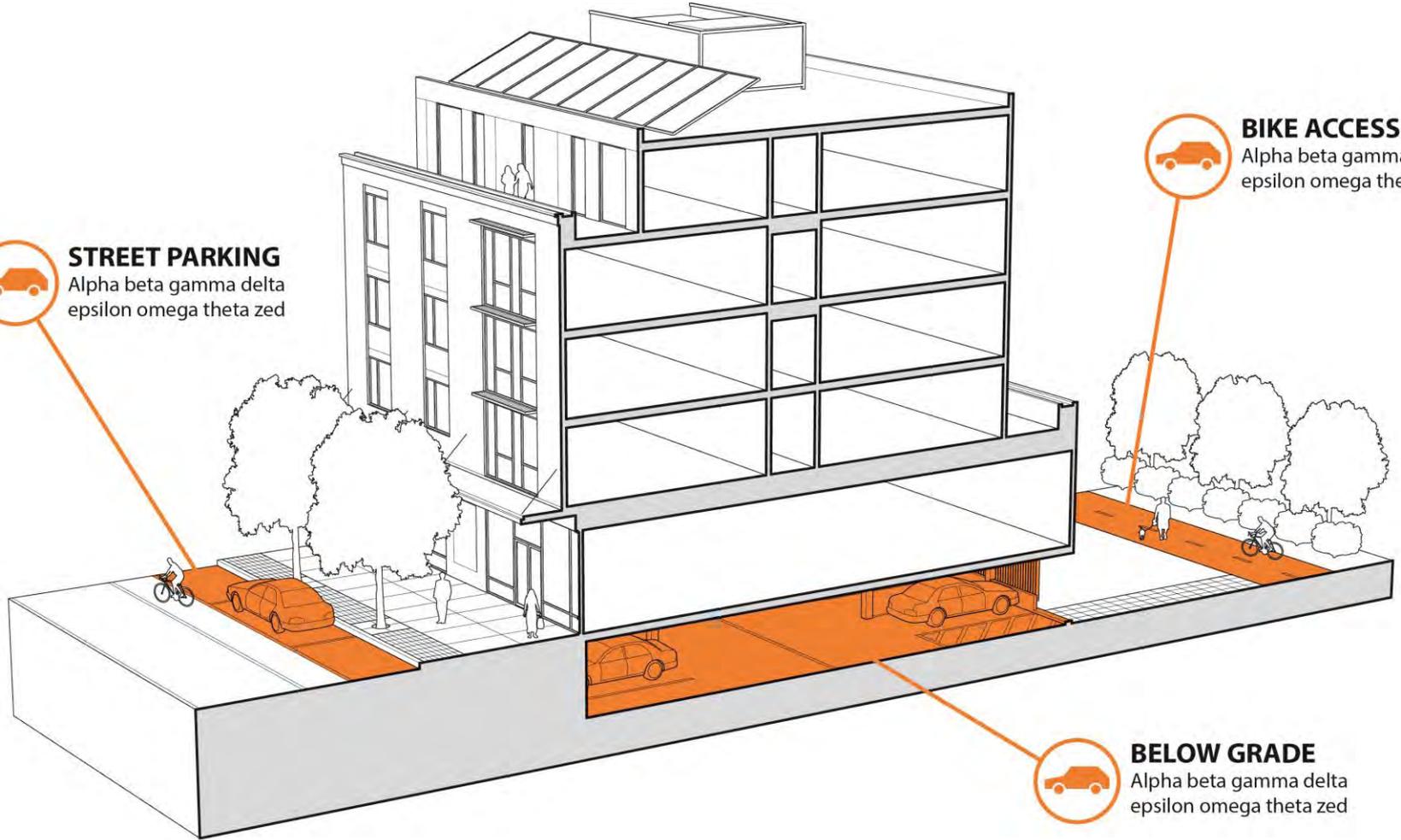
STREET PARKING

Alpha beta gamma delta
epsilon omega theta zed



BIKE ACCESS

Alpha beta gamma delta
epsilon omega theta zed



BELOW GRADE

Alpha beta gamma delta
epsilon omega theta zed



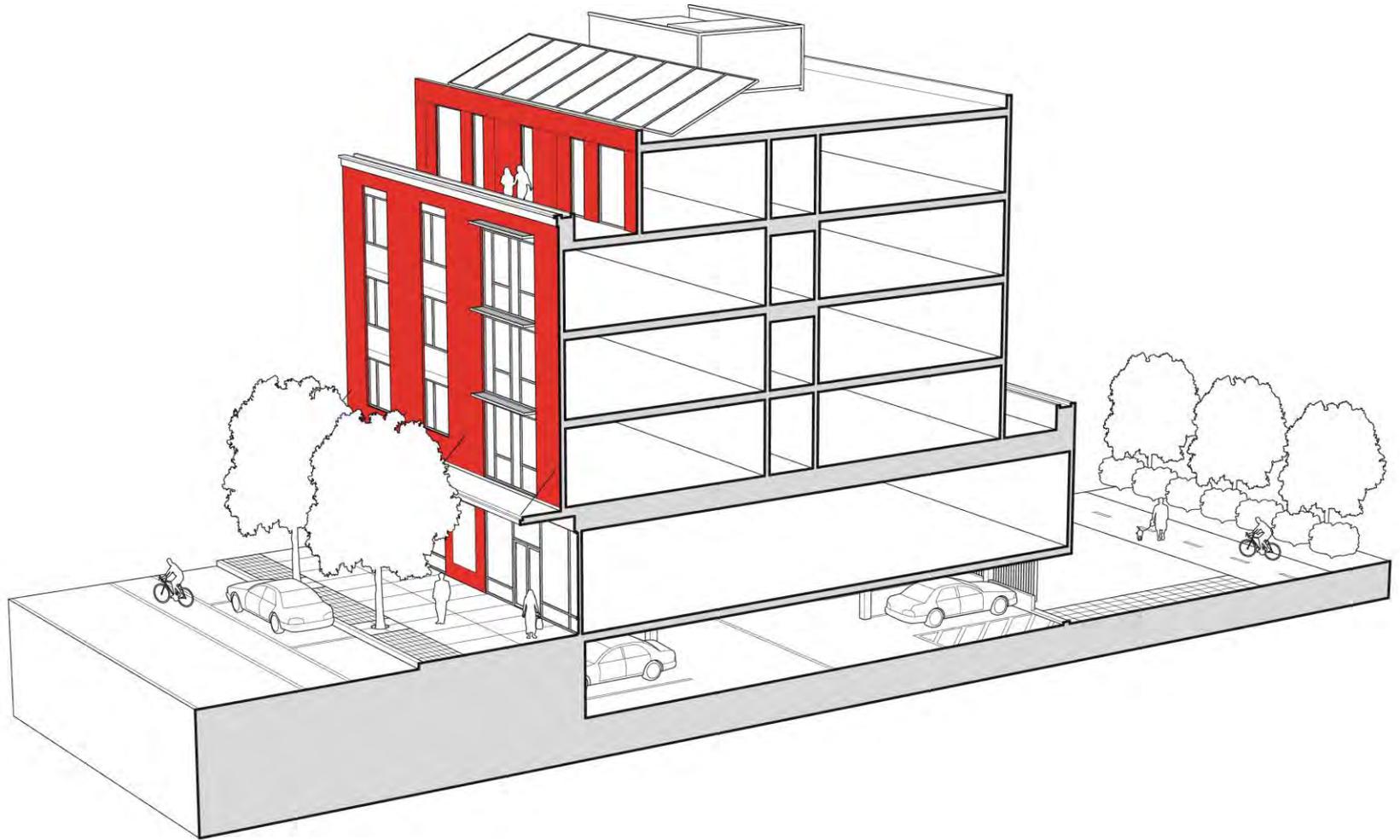
PARKING + ACCESS



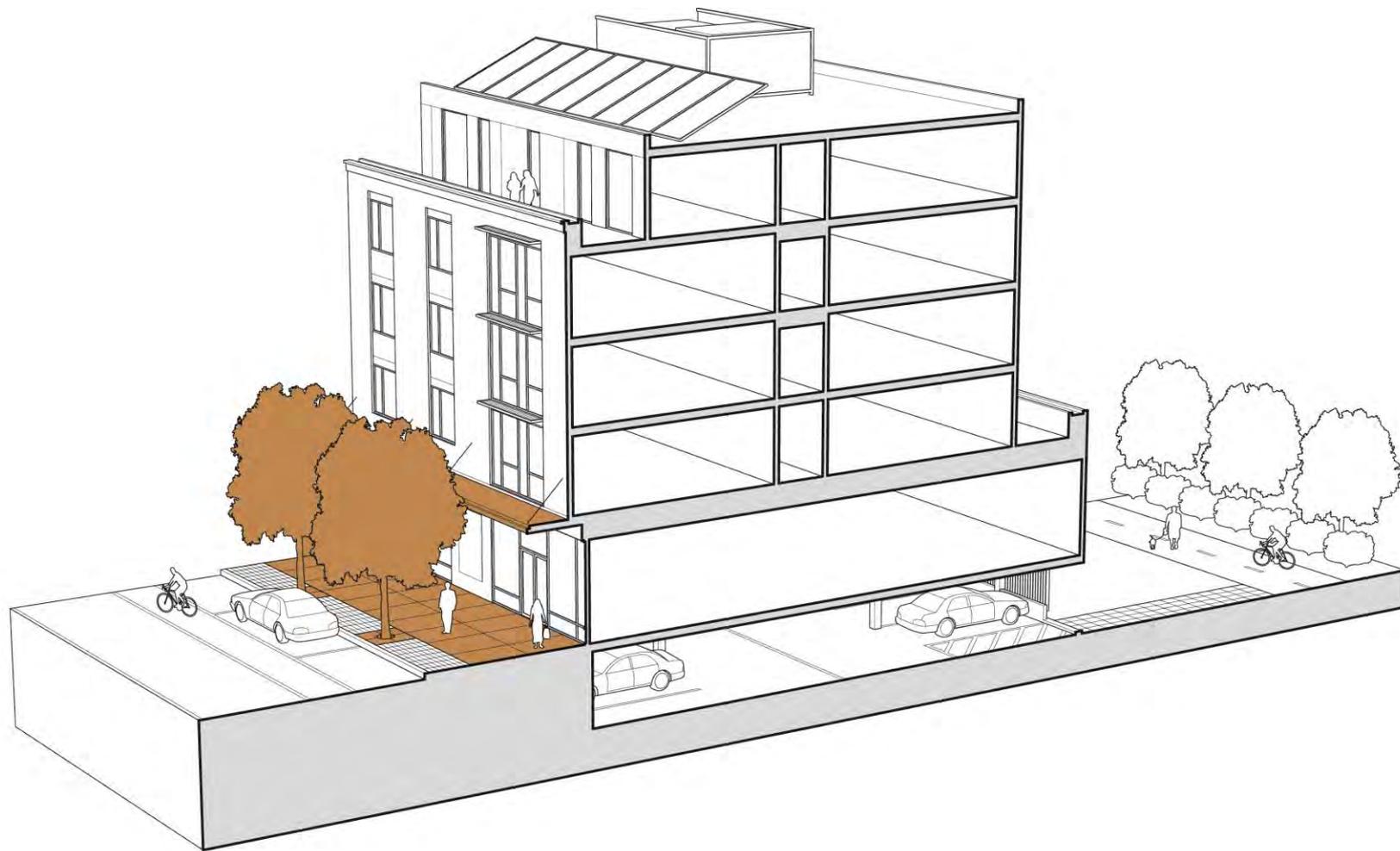
FACADE TREATMENT



BUILDING MASSING



MATERIAL SELECTION

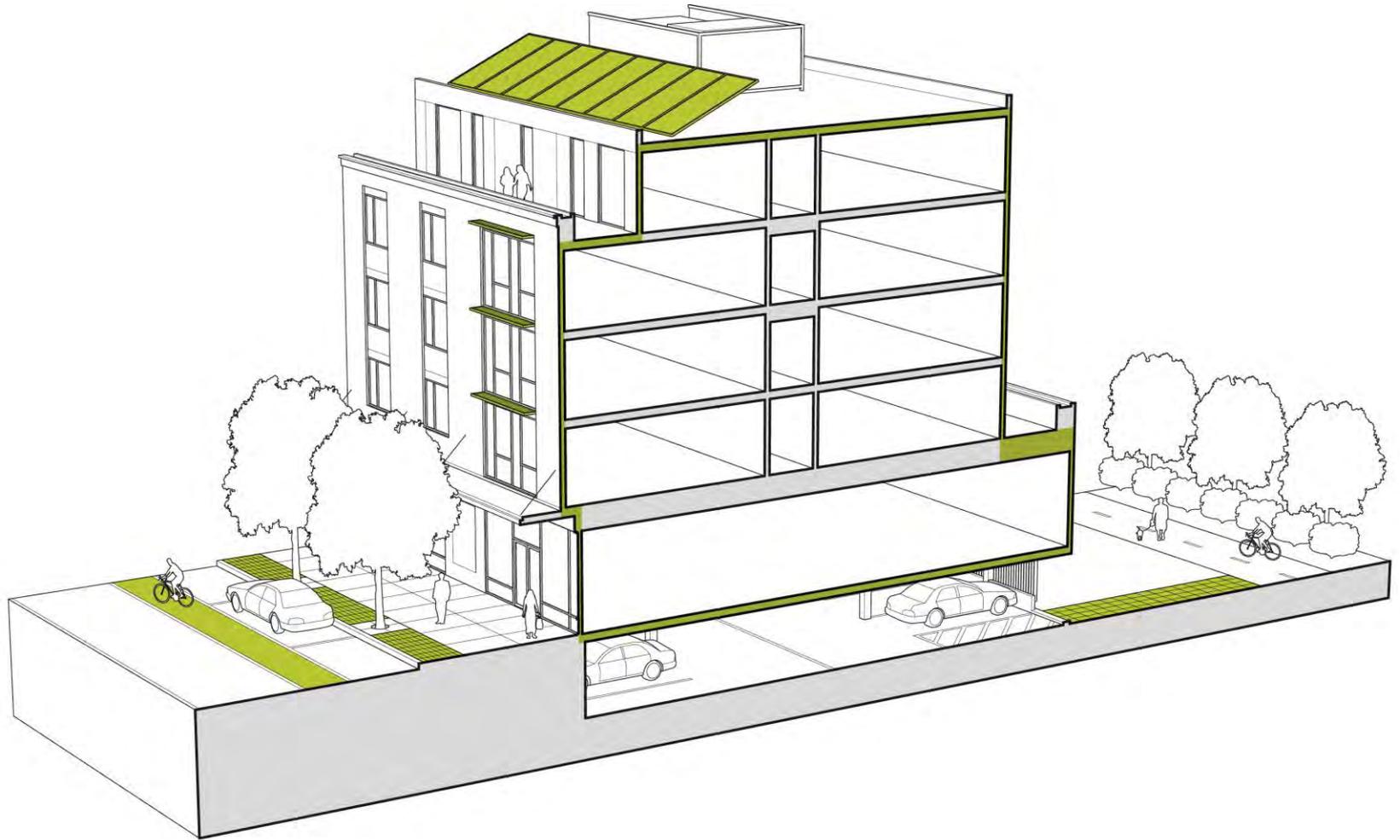


PUBLIC REALM INTERFACE

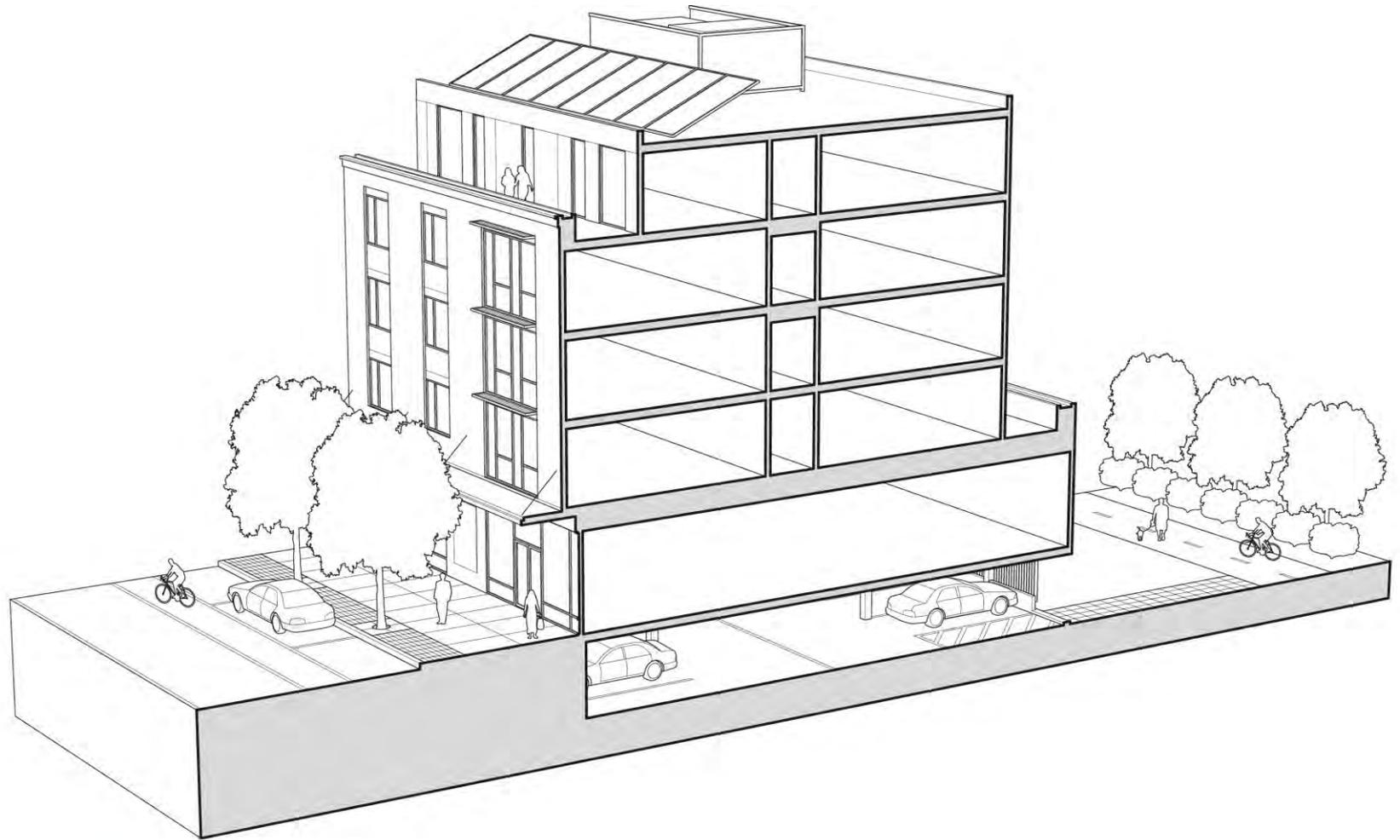


 **BUILDING SETBACKS**

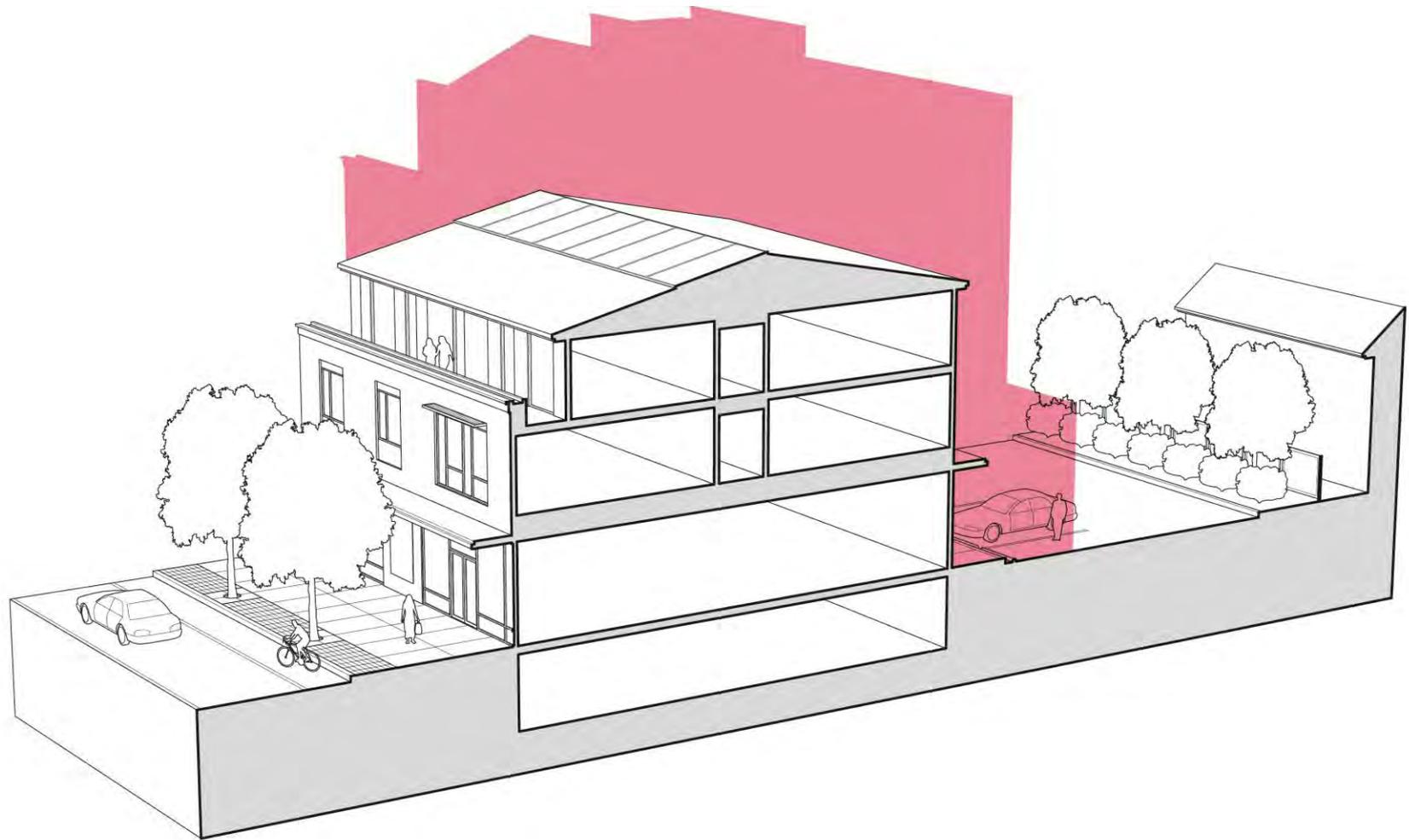




SUSTAINABLE DESIGN



BUILDING HEIGHT



BUILDING HEIGHT

WATERTOWN DESIGN GUIDELINES + STANDARDS



PUBLIC MEETING 1

Watertown Middle School

October 15, 2014

architecture
urban design

GAMBLE
ASSOCIATES